

FINAL PARKING STUDY

for the

Conversion of 1,203 SF of Hallway Space Use to Office/Retail Use

for the

Hermosa Beach Pavilion at 1601 Pacific Coast Highway



PRESENTED TO:



City of Hermosa Beach
Community Development Department
Civic Center – 1315 Valley Drive
Hermosa Beach, CA 90254



PREPARED BY:



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30 Years of Excellence

2/19/2025

**FINAL PARKING STUDY
FOR
CONVERSION OF 1,203 SF OF HALLWAY SPACE USE TO OFFICE/RETAIL USE
FOR THE
HERMOSA BEACH PAVILION
AT
1601 PACIFIC COAST HIGHWAY**

**Prepared for:
CITY OF HERMOSA BEACH**

**Date:
FEBRUARY 19, 2025**

I, Fred Minagar do hereby certify that this Parking Study was performed under my supervision. The enclosed memorandum summarizes the results of a parking utilization survey conducted for the existing Hermosa Beach Pavilion site located at 1601 Pacific Coast Highway, in the City of Hermosa Beach. The parking study identifies the existing parking supply and demand, and addresses the potential parking impacts associated with the requested conversion of 1,203 SF of Hallway Space Use to Office/Retail Use.

The following analyses were performed in compliance with local, state, national and industry wide standard principles, methodologies and best practices for parking accumulation and utilization studies, and shared parking demand forecasting. The field survey data presented in this report illustrate the true and accurate conditions of the HBP parking structure study site during the time periods described herein.



Fred Minagar, MS, RCE, PE, Registration No. 53466
Project Manager

MINAGAR & ASSOCIATES, INC.

ITS - Traffic/Civil/Electrical Engineering - Transportation Planning - Homeland Security - CEM

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- A Proposed Hermosa Beach Pavilion Conversion to Office/Retail Use Site Plans
- B City of Hermosa Beach Applicable Parking Rates (Municipal Code)
- C As-Built Parking Plan
- D Parking Site Occupancy Data





1.0 INTRODUCTION

Minagar & Associates, Inc. was requested by the City of Hermosa Beach to conduct a parking utilization study for the Hermosa Beach Pavilion (HBP), a Mixed-Use commercial development located at 1601 Pacific Coast Highway (PCH) at the corner of PCH and 16th Street, in the City of Hermosa Beach. The HBP is a two-story building and home to various office, retail, and health & fitness businesses operating out of leased suites on both floors. The vicinity of the site with respect to the local street system is shown in Figure 1-1.

The HBP owner/manager is planning to convert 1,203 square feet of first floor common area to spec suites (office/retail use). For reference, a summary of the existing suites within the building is provided below:

Existing-Occupied (92,347 square feet):

- 1st floor: Mixed-Use office and retail accumulating an area of 58,999 square feet (suites 101, 145, 150, 180).
- 2nd floor: Mixed-Use office and retail accumulating an area of 33,438 square feet (suites 245, 265-B, 280, 285, E)

Existing-Vacant (24,236 square feet):

- 1st floor: Mixed-Use office and retail suites accumulating an area of 13,170 square feet (suites 140, 155, 160, 165, 170, and 175)
- 2nd floor: Mixed-Use office and retail suites accumulating an area of 11,066 square feet (suites 265-A, E)

Minagar & Associates, Inc. conducted a study to evaluate whether the HBP can sufficiently accommodate the additional parking demands caused by the conversion of 1,203 square feet Hallway Space Use to Office/Retail Use with respect to the existing parking conditions at the HBP.

The HBP currently provides a cumulative 128,089 rentable square feet, or 116,583 square feet usable gross floor area. Attached to the main building is a six-level parking structure with covered parking spaces for employees and customers of the HBP. The parking structure contains 496 marked parking spaces and provides elevators, escalators, and stairwells at various levels to allow patrons to access the main storefront.

As shown in Figure 1-2, vehicular access to the parking structure is provided from two entrance-ways on 16th Street.





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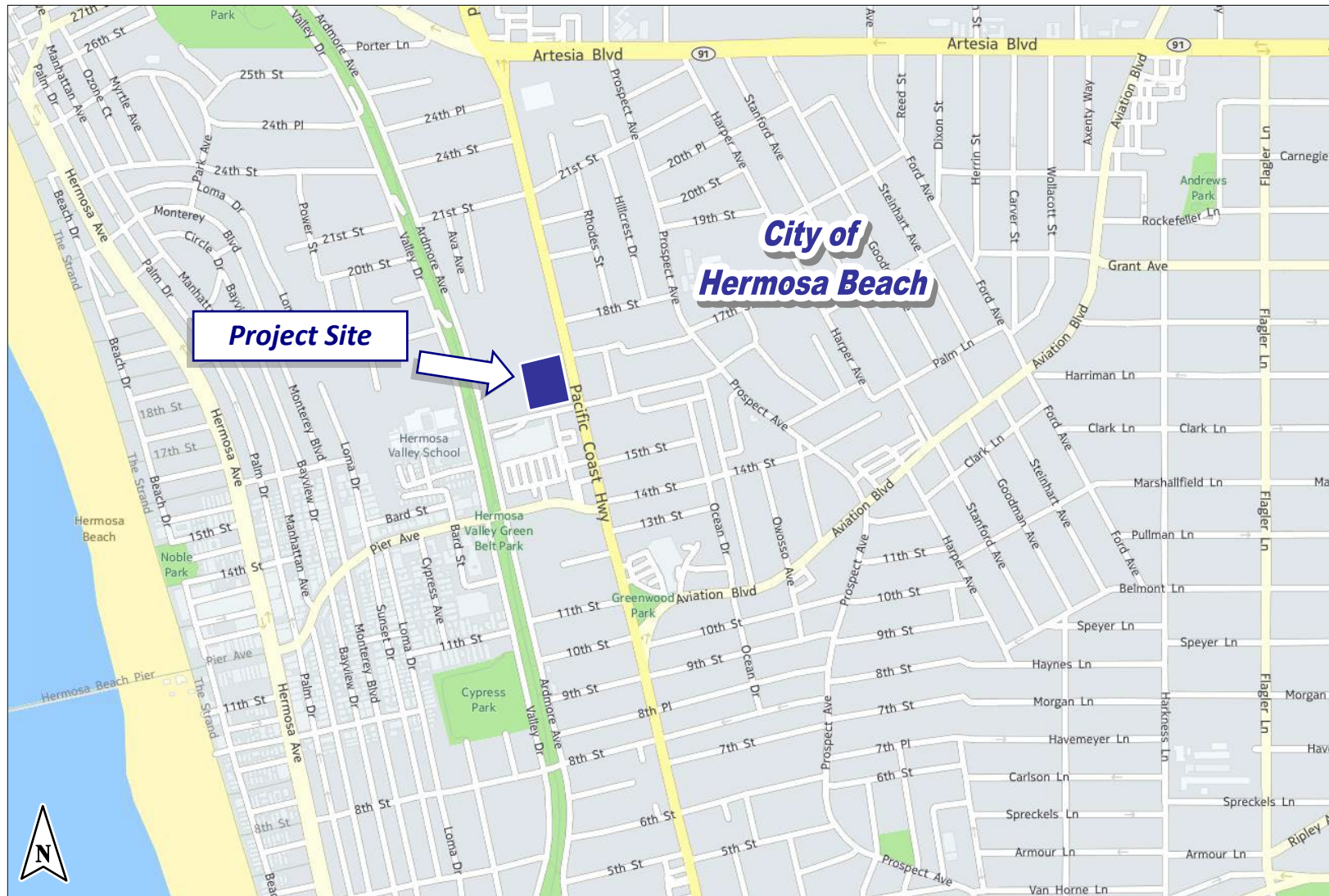
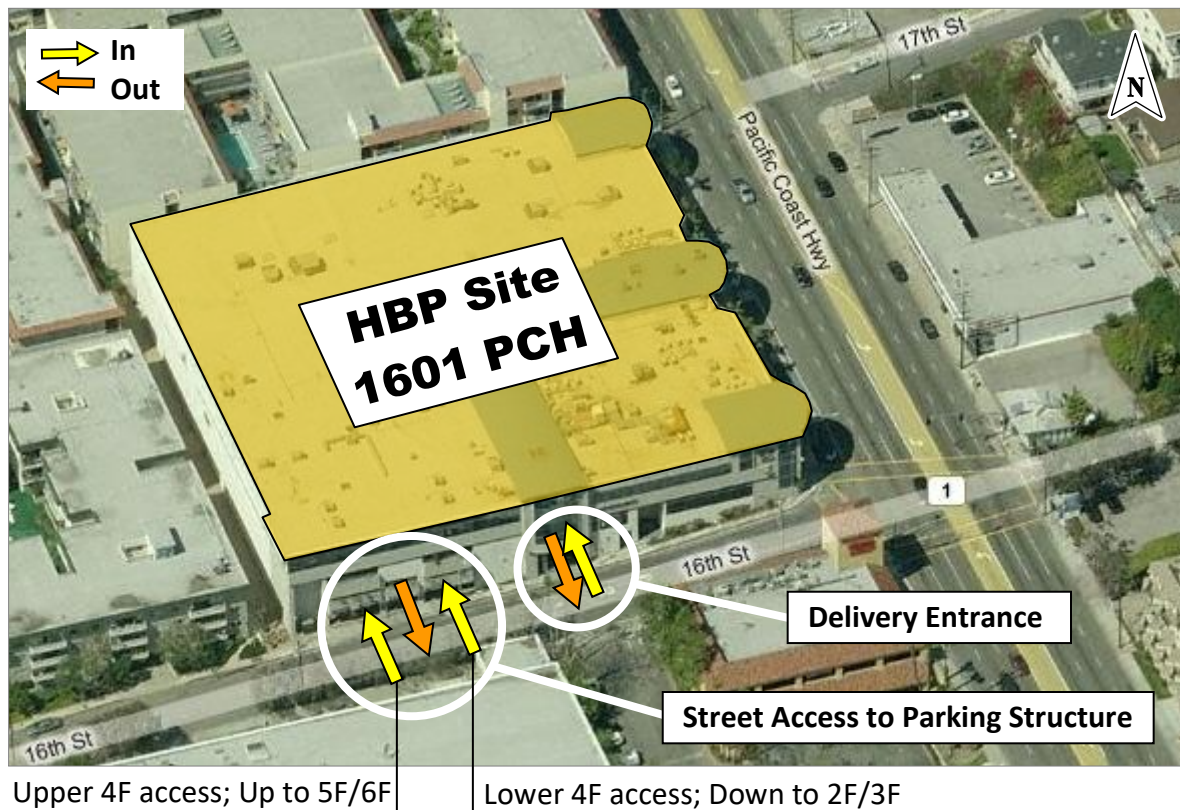


Figure 1-1. Vicinity Map





Figure 1-2. Project Site



Minagar & Associates, Inc. conducted parking accumulation surveys at the existing parking structure site, estimated the probable shared-use parking demand of proposed developments at the HBP, and then compared the results with the City's Municipal/Parking Code requirements.

The results of the parking analyses are described in the sections below. Table 1-1 summarizes the existing and proposed uses of the HBP site.



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**TABLE 1-1: Shared Parking Demand
 Existing and Proposed Tenant Use by Suite***

Suite No.	EXISTING SUITE		Size (SF)	PROPOSED CONVERSION TO OFFICE/RETAIL USE	Size (SF)
	Tenant	Land Use		Tenant	
101	24-Hour Fitness - Occupied	Health & Fitness	50,765	N/C	
140	Vacant		1,662	N/C	
145	Absolute Laser Tattoo Removal - Occupied	Retail	1,662	N/C	
150	Pacific Crest Multi Specialty Surgery - Occupied	Medical Office	3,702	N/C	
155	Vacant		1,102	811 SF OF HALLWAY SPACE	
160	Vacant		831	N/C	
165	Vacant		1,480	392 SF OF HALLWAY SPACE	
170	Vacant		3,357	N/C	
175	Vacant		4,738	N/C	
180	South Bay Golf Club - Occupied	Health & Fitness	2,870	N/C	
245	Walker Law Firm - Occupied	Office	2,865	N/C	
265-A	Vacant		4,800	N/C	
265-B	Fusion Academy - Occupied	Office	6,478	N/C	
280	Salon Republic - Occupied	Service ^[2]	16,179	N/C	
285	Massage Envy - Occupied	Health Center	3,666	N/C	
E	(Various Offices) - Vacant	Office	6,266	N/C	
E	(Various Offices) - Occupied	Office	4,160	N/C	
Occupied Floor Area (Usable SF):		Existing-Occupied:	92,347	Proposed Conversion:	1,203

N/C: No Change

*The list is updated as of February 5, 2025 Rent Roll

Vacant/For Lease

[1]Per Hermosa Beach's Parking Code 17.44.030-0 (October 2021) 1/250 (general retail commercial) used in place of restaurant (1/100).

[2]"Service" Use based on related uses per Hermosa Beach's Parking Code 17.44.030, such as General Retail (1/250), General Office (1/250), and Gym/Health & Fitness Center <3,000 SF (1/250).

[3] 1,203 Square Feet is the Total Area of Proposed Conversion (Occupied+Vacant) per Plan Set in Appendix A

[4] 92,347 Square Feet is the Total Area of Existing-Occupied (Proposed Conversion + Unchanged) Suites





2.0 EXISTING CONDITIONS

2.1 Existing Parking Supply

The Hermosa Beach Pavilion site provides 496 parking spaces within the existing parking structure. Parking spaces consist of a mix of standard and compact sized, handicap accessible stalls, motorcycle stalls, designated spaces for bicycles with bike racks, and several signed/marked stalls for customers and employees of specific tenants within the HBP. A breakdown of the total number of available marked parking spaces at each level of the parking structure is listed below:

Table 2-0

Parking Availability at the HBP Parking Structure

■ Level 2	122 spaces
■ Level 3	118 spaces
■ Level 4	81 spaces
■ Level 5	93 spaces
■ Level 6	82 spaces
<hr/>	
Total:	496 available spaces

The parking structure is accessible at the street level at Level 4 via the public parking entrances on 16th Street. Parking Levels 2 and 3 are located underground, while Levels 5 and 6 are located aboveground.

2.2 Existing Requirement Per City Code

In accordance with the City of Hermosa Beach’s Municipal Code, Chapter 17.44 (030), “Off Street Parking-Commercial and Business Uses,” the owner of the site must provide the minimum number of required parking spaces to accommodate all of the site’s existing and anticipated proposed parking demands. This requirement is determined initially on the basis of the City’s Parking Code, which provides a schedule of parking rates to apply to individual land use types. In applying the City’s parking rates to the individual existing uses within the HBP, the cumulative number of required off-street parking spaces calculates to 787 (686 occupied + 96 (theoretical) +5 proposed). Table 2-1, shown below, summarizes this code-based requirement for the existing active tenant uses at the HBP. Subsequently, Table 2-2 highlights the theoretical projected





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parking demand for the existing vacant spaces based on the square footage of the existing suites.

Table 2-1: Existing-Occupied Use Parking Requirement per City Code

EXISTING SITE				Required Parking (496 provided on-site)	
Ste.	Tenant	Land Use	Size (SF)	Parking Ratio ^[1]	# of Spaces
101	24-Hour Fitness	Gym/health center	50,765	1 space per 100 SF	508
145	Absolute Laser Tattoo Removal	Retail	1,662	1 space per 250 SF	7
150	Pacific Crest Multi Specialty Surgery	Office (Medical)	3,702	1 space per 200 SF	19
180	South Bay Golf Club	Gym/health center	2,870	1 space per 250 SF	12
245	Walker Law Firm, Inc	Office	2,865	1 space per 250 SF	12
265-B	Fusion Academy	Office (General)	6,478	1 space per 250 SF	26
280	Salon Republic	Service ^[2]	16,179	1 space per 250 SF	65
285	Massage Envy	Health Center	3,666	1 space per 250 SF	15
E	(various office suites)	Office (General)	4,160	1 space per 250 SF	17
New	Office Suites	Office (General)	1,203	1 space per 250 SF	5
Parking Requirement, Existing Uses (City Code based): 686 Spaces (28.0% shortage)					
Actual Peak Parking Demand ^[3] : 433 Spaces (13.7% surplus)					

^[1] Per City of Hermosa Beach Municipal/Parking Code

^[2] "Service" Use based on related uses per Hermosa Beach's Parking Code 17.44.030 (October 2021), such as General Retail (1/250), General Office (1/250), and Gym/Health & Fitness Center <3,000 SF (1/250).

^[3] Based on peak-day/peak-hour parking accumulation surveys conducted at the HBP site by Metropolis

Table 2-2: Existing-Vacant Use Parking Requirement (Theoretical)

EXISTING SITE				Required Parking (496 provided on-site)	
Ste.	Tenant	Land Use ^[2]	Size (SF)	Parking Ratio ^[1]	# of Spaces
140	Vacant	N/A	1,662	1 space per 250 SF	7
155	Vacant	N/A	1,102	1 space per 250 SF	4
160	Vacant	N/A	831	1 space per 250 SF	3
165	Vacant	N/A	1,480	1 space per 250 SF	6
170	Vacant	N/A	3,357	1 space per 250 SF	13
175	Vacant	N/A	4,738	1 space per 250 SF	19
265-A	Vacant	N/A	4,800	1 space per 250 SF	19
E	Vacant	N/A	6,266	1 space per 250 SF	25
Total Projected Parking Demand: 96 Spaces					





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-
- [1] Assumption of 1 space per 250 SF based on City of Hermosa Beach's Parking Code 17.44.030 General Office (1/250)
[2] Land Use not available due to vacancy status

Given the supply of 496 spaces and the calculated requirement for 787 (686 occupied + 96 (theoretical) vacant) spaces, there is a shortfall of 291 spaces. ***This shortfall is based on theoretical/city code data and represents current uses, intended to show the minimum demand for additional parking spaces based strictly on the City of Hermosa Beach's Municipal Code parking rates. It does not reflect growth, increases in demand, or the actual field-survey demand as discussed below.***

Due to the lack of land use data, a parking ratio of 1 space per 250 SF was assumed per the municipal code for all vacant suites for the purpose of this calculation.

2.3 Existing Weekday & Weekend Parking Utilization

Minagar & Associates, Inc. performed a comprehensive parking utilization analysis utilizing hourly occupancy data provided by Metropolis. The study encompassed a 21-day period from January 15, 2025, to February 5, 2025, with continuous 24-hour daily monitoring.

According to the provided site occupancy data:

7-day peak occupancy (occurred at 10:00 AM on a Saturday): 433 spaces

Corresponding available spaces during the peak occupancy: 496

The analysis reveals a contrast between the City's Municipal Code parking requirements and the actual observed peak usage. While the code-based calculations indicate a parking shortage, the real-world utilization data demonstrates a marginal surplus in parking capacity. This disparity highlights the effectiveness of shared parking in Mixed-Use developments like the Hermosa Beach Pavilion, where complementary peak hours among different uses allow for more efficient use of parking resources.





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The Mixed-Use commercial center's shared parking utilization on typical weekdays ranged from 18.15% to 79%, with an overall peak parking utilization/occupancy of 79.03% (392 out of 496 spaces occupied). **This weekday peak utilization, typically occurring around 4:00 p.m. on weekdays, leaves 21.97% of spaces available even during periods of highest demand. The data shows a surplus of 104 spaces during peak demand, representing a 21.97% surplus in parking capacity (not accounting for the parking demand generated by future occupied vacant suites.)**

The Mixed-Use commercial center's shared parking utilization on typical weekends ranged from 24.40% to 87%, with an overall peak parking utilization/occupancy of 87.3% (433 out of 496 spaces occupied). **This weekend peak utilization, typically occurring around 10:00 a.m. on weekends, leaves 12.7% of spaces available even during periods of highest demand. The data shows a surplus of 63 spaces during peak demand, representing a 12.7% surplus in parking capacity based on the existing land use/space occupancy, existing parking supply (not accounting for the parking demand generated by future occupied vacant suites.)**

This observed parking behavior demonstrates the benefits of Mixed-Use development in optimizing parking utilization. The staggered peak hours of various uses within the Hermosa Beach Pavilion contribute to a more balanced and efficient use of the parking facilities throughout the day. Figures 2-1 and 2-2 provide a visual summary of the existing parking utilization data for the site.



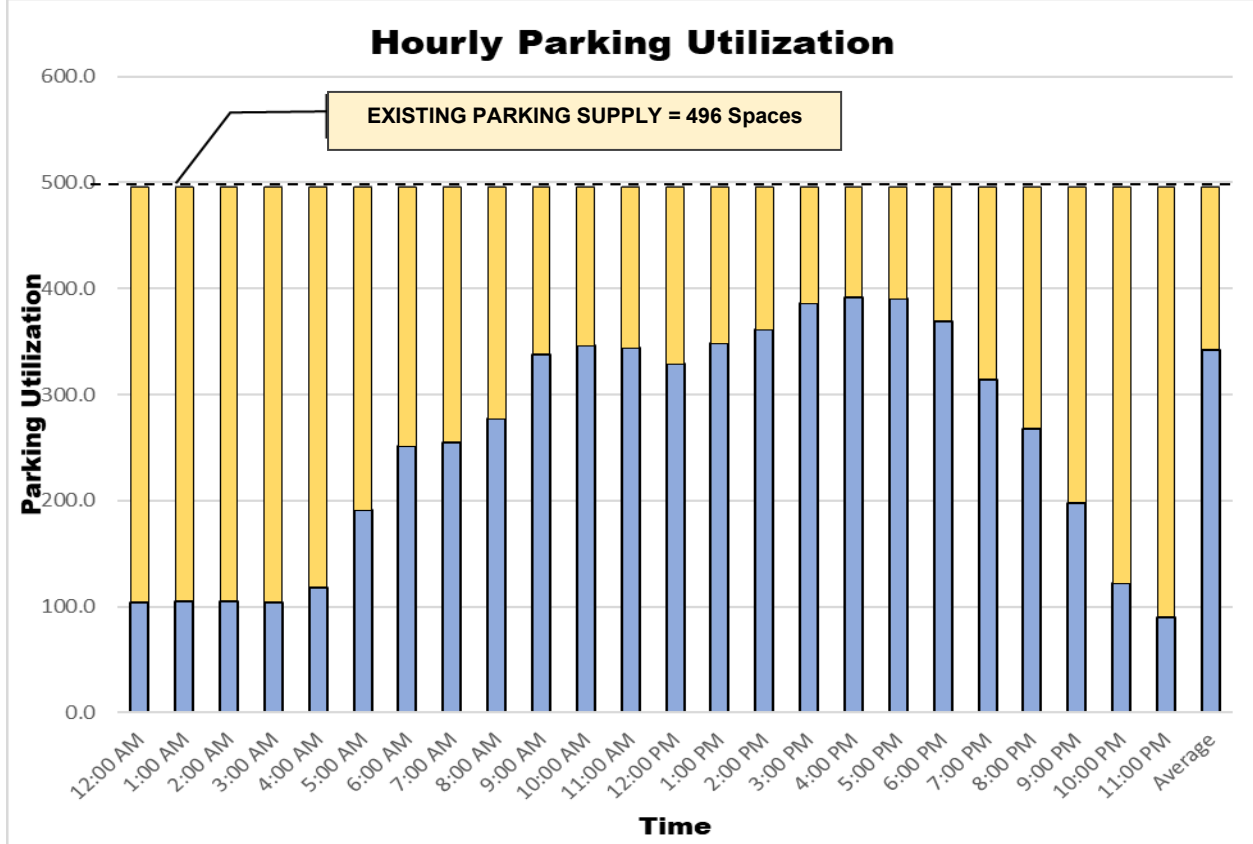


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FIGURE 2-1 WEEKDAY PARKING UTILIZATION



Time	Occupied Parking Spaces	Vacant Parking Spaces	Percent Occupancy:
7:00 AM	255.0	241.0	51.41%
8:00 AM	277.0	219.0	55.85%
9:00 AM	338.0	158.0	68.15%
10:00 AM	346.0	150.0	69.76%
11:00 AM	344.0	152.0	69.35%
12:00 PM	329.0	167.0	66.33%
1:00 PM	348.0	148.0	70.16%
2:00 PM	361.0	135.0	72.78%
3:00 PM	386.0	110.0	77.82%
4:00 PM	392.0	104.0	79.03%
5:00 PM	390.0	106.0	78.63%
6:00 PM	369.0	127.0	74.40%
7:00 PM	314.0	182.0	63.31%
Average	342	154	69.00%



Peak Hour

Weekday 7AM-7PM Parking Utilization



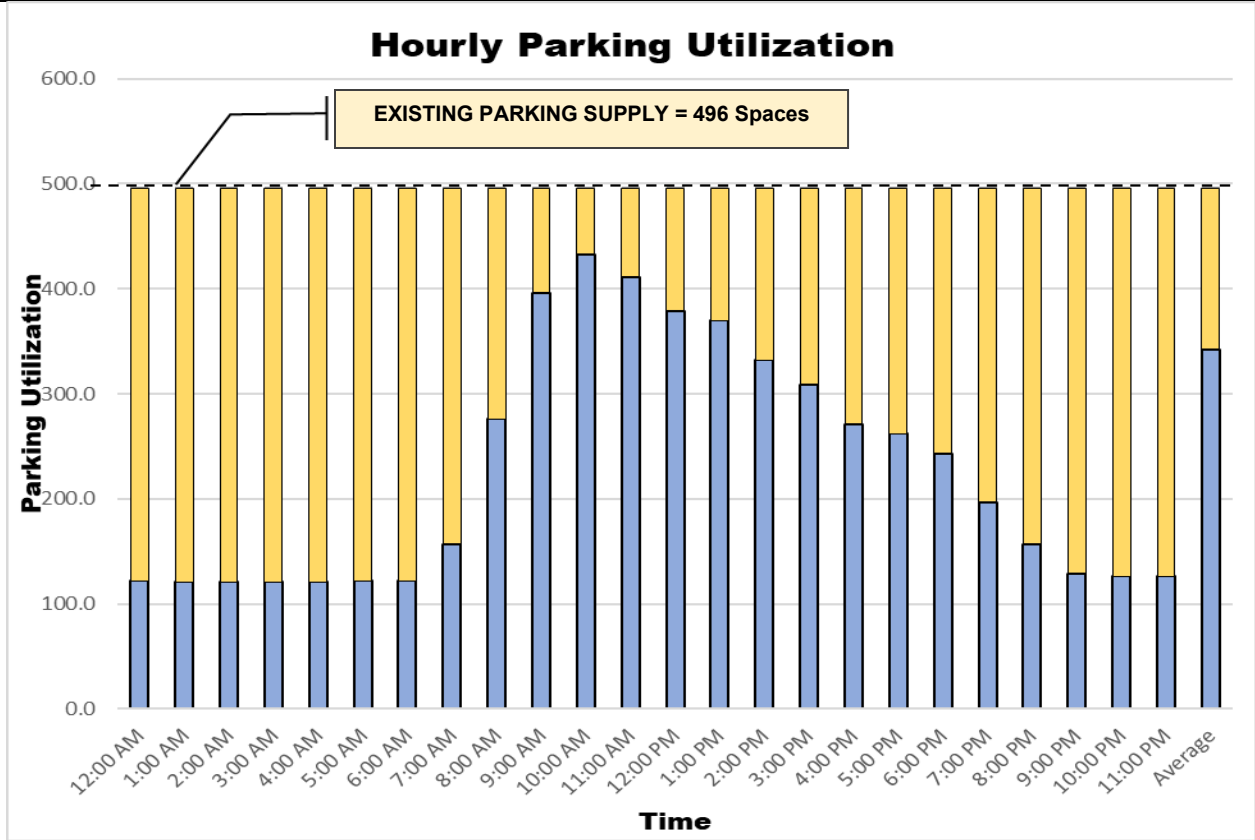


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FIGURE 2-2 WEEKEND PARKING UTILIZATION



Time	Occupied Parking Spaces	Vacant Parking Spaces	Percent Occupancy:
7:00 AM	157.0	339.0	31.65%
8:00 AM	276.0	220.0	55.65%
9:00 AM	396.0	100.0	79.84%
10:00 AM	433.0	63.0	87.30%
11:00 AM	411.0	85.0	82.86%
12:00 PM	379.0	117.0	76.41%
1:00 PM	370.0	126.0	74.60%
2:00 PM	332.0	164.0	66.94%
3:00 PM	309.0	187.0	62.30%
4:00 PM	271.0	225.0	54.64%
5:00 PM	262.0	234.0	52.82%
6:00 PM	243.0	253.0	48.99%
7:00 PM	197.0	299.0	39.72%
Average	311	186	62.59%

Peak Hour

Weekend 7AM-7PM Average Parking Utilization





2.4 Shared Parking Demand at the Hermosa Beach Pavilion

The City of Hermosa Beach offers a multitude of attractions for visitors. During the Spring and Summer seasons, particularly on sunny days and on weekends, locals and visitors are drawn to the beach located just a few blocks to the west of Pacific Coast Highway. Beachgoers also visit local retail stores, restaurants, conduct business and vice versa. Thus, multiple land uses are visited, yet only one parking space is needed. This type of shared parking activity, where the use of a parking space serves two or more individual land uses without conflict or encroachment, is seen at the Hermosa Beach Pavilion as a result of two key observed factors:

- 1) The complementary relationships between land uses within the HBP, resulting in multiple trip purposes for visitors on the same "parked" trip (e.g., office employees using the 24-hour fitness upon arriving or departing from the HBP, etc.);
- 2) The demand for HBP uses varied during different times of the day. For example, the 24-hour fitness experienced peak patronage before and after typical weekday work hours. When considered for all of the different uses within the center, these arrival variations and staggered business activities lend to the shared parking qualities of the HBP.

Mixed-Use developments like the HBP are often able to capitalize on the benefits of shared parking by developing a variety of diverse, but complementary land use types on a single site. This usually results in a parking demand for the overall site that is appreciably lower than the sum of individual parking demands for each land use when considered as separate, freestanding developments. The Institute of Transportation Engineers (ITE) calls these types of trips with primary/secondary visit purposes "internal trips/capture," for which trip and parking reductions can be taken.

In reducing the number of parking spaces required to such uses, shared parking provides spatial benefits to the community, especially for the surrounding built-out beach neighborhood and downtown district of Hermosa Beach where public parking during peak periods comes at a premium. After conducting several field visits and reviewing the proposed tenant uses of the Hermosa Beach Pavilion, it was determined that the site does currently provide, and will continue to provide, opportunities for shared





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parking resulting in the need to consider an appropriate discount to the overall parking requirement to reflect more realistic shared parking demands.

While the relationship between complementary land uses might reduce the overall parking demand, there are limits to the influence of captive market effects, such as the relative proximity of land uses to one another, and also to available parking facilities. Minagar & Associates, Inc.'s field visits, however, verified that with respect to such physical constraints there are indeed high incentives for shared parking at the HBP, due to several factors such as:

- Connectivity and proximity of each land use within the HBP. The shops are located side-by-side and enclosed in an indoor mall type of environment;
- Direct indoor access from the parking structure to the shops, and vice versa, within the HBP;
- Use of a vertical parking structure that minimizes walk times from user vehicles to the shops, and the added convenience for visitors and employees of being able to access each level via escalator, elevator or stairwell; and
- Convenient pedestrian accessibility to the HBP from adjacent streets and sidewalks.

As described in Section 3.3.2, a shared parking analysis and forecast was conducted using the above observations, in order to estimate a more realistic potential for peak shared parking demand at the Hermosa Beach Pavilion under the proposed condition.





3.0 PROPOSED PARKING CONDITIONS

3.1 Proposed Conversion of 1,203 SF Hallway Space Use to Office/Retail Use

The HBP owner/manager is planning to convert 1203 square feet of first floor common area to spec suites (office use). The proposed action plan will impact the following suites (per plan set provided in **Appendix A**):

Existing-Vacant (6,770 square feet):

- 1st floor: Mixed-Use office and retail suites (suites 155, 160, 165, 170)

3.2 Parking Requirement: Proposed Conversion of 1203 SF Hallway Space Use to Office/Retail Use

3.2.1 Required Parking per City Code

The City of Hermosa Beach's Parking Code (October 2021) requires that a site's proposed parking demand be determined on the basis of square footage, in accordance with established parking ratios and the proposed land use types for the project site. Using the City's municipal code parking rate requirements for the conversion of Mixed-Use Space Hermosa Beach Pavilion to an Office Use Space, the HBP will be required to provide an additional 5 parking spaces to accommodate the proposed 1203 SF office conversion.

The total required parking for the HBP site under the proposed conditions, calculated by adding the estimated City Code-based parking requirement for the proposed use to the baseline observed peak parking demand of the site, is therefore 534 parking spaces (433 peak weekend demand + 96 (vacant) + 5 new spaces). This estimation method represents the state-of-the-practice for evaluating the updated parking requirement of a Mixed-Use development, and is the accepted practice in beach communities of Los Angeles County, Orange County, and other coastline cities throughout the State of California.





3.2.2 Shared Parking Requirement (ULI Shared Parking Analysis)

A shared parking analysis was conducted to provide an alternative, more practical estimate of the projected peak parking demands of the HBP. As stated in Section 2.4, multiple field visits, site observations and parking survey data collected at the HBP confirmed that the Pavilion experiences shared parking characteristics on typical weekdays and weekends. Due to the location and complementary nature of the interior office, retail and service businesses, the HBP under the proposed condition will continue to be consistent with the Mixed-Use nature of the site and benefit from shared parking.

Empirical data on this has been collected by ITE, and shared time-of-day parking rates have been developed for specific land uses. The Urban Land Institute (ULI) Shared Parking Manual was used to evaluate time-of-day parking reductions for the HBP under the proposed condition. This methodology first estimates the "stand-alone" peak parking requirements for each land use, then applies a combination of reduction factors to account for shared parking effects such as seasonality, time-of-day variations, modal splits—and as needed—locally-developed parking requirements and considerations. Each of these factors is then incorporated into pre-developed calculation worksheets which apply the factors to the selected land uses comprising the development. A projected peak shared parking requirement is then calculated for the peak time-of-day, day-of-week, and season during the year in which shared parking activity at the site will reach a maximum.

For this analysis, Minagar & Associates, Inc. considered the "proposed" site condition to consist of the pre-existing peak period baseline demand (based on the field-measured parking accumulation surveys from Metropolis) parking generation estimates were determined using ULI- and ITE-derived parking data for each land use.

Table 3-1 depicts the theoretical parking demand generated by the vacant suites based on the ULI parking demand ratio of 1 space for every 350 SF of office use.

Table 3-2, below, summarizes the results of the shared parking analysis for the proposed condition **excluding** the parking demand for vacant suites. The total number of shared parking spaces for each land use is listed in the table.





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Table 3-1: Existing-Vacant Use Under Shared Parking Requirement (Theoretical)

EXISTING SITE				Required Parking (496 provided on-site)	
Ste.	Tenant	Land Use	Size (SF)	Parking Ratio	# of Spaces
140	Vacant	N/A	1,662	1 space per 350 SF	4
155	Vacant	N/A	1,102	1 space per 350 SF	3
160	Vacant	N/A	831	1 space per 350 SF	2
165	Vacant	N/A	1,480	1 space per 350 SF	4
170	Vacant	N/A	3,357	1 space per 350 SF	9
175	Vacant	N/A	4,738	1 space per 350 SF	13
265-A	Vacant	N/A	4,800	1 space per 350 SF	13
E	Vacant	N/A	6,266	1 space per 350 SF	18
Total Projected Parking Demand: 66 Spaces					

TABLE 3-2: Shared Parking Demand
Proposed Conversion of 1,203 SF Hallway Space Use to Office/Retail Use
(excluding existing vacant office suites)

LAND USE	Existing-Occupied HBP Site		Hallway Use HBP Conversion to Office Use					TOTAL SHARED PARKING DEMAND
Size	92,347 SF		1,203 SF					
Unit Land Use	-		Visitor	Employee	Peak Total			
Parking Ratio	-		0.3 /ksf	3.5/ksf	3.8/ ksf			
Gross Demand	433		0 spaces		5 spaces		5 spaces	
	Existing % of Peak	Total Number of Spaces	% of Peak	# of Spaces	% of Peak	# of Spaces	Total # of Spaces	
7:00 AM	36.26%	157	20%	0	20%	1	1	158
8:00 AM	63.74%	276	60%	0	60%	3	3	279
9:00 AM	91.45%	396	80%	0	80%	4	4	400
10:00 AM	100.00%	433	90%	0	90%	5	5	438
11:00 AM	94.92%	411	100%	0	100%	5	5	416
12:00 PM	87.53%	379	90%	0	90%	5	5	384
1:00 PM	85.45%	370	80%	0	80%	4	4	374
2:00 PM	76.67%	332	60%	0	60%	3	3	335
3:00 PM	71.36%	309	40%	0	40%	2	2	311
4:00 PM	62.59%	271	20%	0	20%	1	1	272
5:00 PM	60.51%	262	10%	0	10%	1	1	263
6:00 PM	56.12%	243	5%	0	5%	0	0	243
7:00 PM	45.50%	197	0%	0	0%	0	0	197

As shown in Table 3-2, the peak shared parking demand occurs at 10:00 AM, with a total of 438 spaces required. This includes 433 spaces from existing occupied uses and 5 spaces from the new 1,203 SF office/retail conversion. The shared parking analysis





FINAL PARKING STUDY

HERMOSA BEACH PAVILION AT 1601 PACIFIC COAST HIGHWAY
 CONVERSION OF 1,203 SF OF HALLWAY SPACE USE TO OFFICE/RETAIL USE

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demonstrates that the HBP can accommodate the proposed changes within the existing 496-space supply, with a surplus of 58 spaces during peak demand. With the addition of the converted HBP space, the HBP will be parked at 88% utilization (438 / 496 spaces).

TABLE 3-3: Shared Parking Demand
Future Condition: Full Occupancy of HBP

LAND USE	Existing-Occupied HBP Site		Existing-Vacant HBP Site		Hallway Use HBP Conversion to Office Use					TOTAL SHARED PARKING DEMAND
	Size	92,347 SF	24,236 SF	1,203 SF						
Unit Land Use	-		-		Visitor	Employee	Peak Total			
Parking Ratio	-		-		0.3 /ksf	3.5/ksf	3.8/ ksf			
Gross Demand	433		66		0 spaces	5 spaces	5 spaces			
	Existing % of Peak	Total Number of Spaces	Existing % of Peak	Total Number of Spaces	% of Peak	# of Spaces	% of Peak	# of Spaces	Total # of Spaces	
7:00 AM	36.26%	157	20%	13	20%	0	20%	1	1	171
8:00 AM	63.74%	276	60%	39	60%	0	60%	3	3	318
9:00 AM	91.45%	396	80%	52	80%	0	80%	4	4	452
10:00 AM	100.00%	433	90%	59	90%	0	90%	5	5	497
11:00 AM	94.92%	411	100%	66	100%	0	100%	5	5	482
12:00 PM	87.53%	379	90%	59	90%	0	90%	5	5	443
1:00 PM	85.45%	370	80%	52	80%	0	80%	4	4	426
2:00 PM	76.67%	332	60%	39	60%	0	60%	3	3	374
3:00 PM	71.36%	309	40%	26	40%	0	40%	2	2	337
4:00 PM	62.59%	271	20%	13	20%	0	20%	1	1	285
5:00 PM	60.51%	262	10%	6	10%	0	10%	1	1	269
6:00 PM	56.12%	243	5%	3	5%	0	5%	0	0	246
7:00 PM	45.50%	197	0%	0	0%	0	0%	0	0	197

The results shown in Table 3-3 reveal that the future peak parking demand will occur at 10:00 AM during the peak weekend, for a total accumulation of 497 spaces. In comparing the findings of the shared parking analysis using the ITE/ULI parking rates, it is seen that the parking demand for the full occupancy condition is not met given the current supply of parking spaces at 496. **The HBP parking structure will operate at a utilization rate of 100.2% during the peak hour with an insignificant deficit of only 1 parking space during the worst hour and day of a typical 7-day week when the entire building is fully occupied.**





FINAL PARKING STUDY

HERMOSA BEACH PAVILION AT 1601 PACIFIC COAST HIGHWAY
 CONVERSION OF 1,203 SF OF HALLWAY SPACE USE TO OFFICE/RETAIL USE

FEBRUARY 19, 2025

3.2.3 Sensitivity Assessment for Rectifying Only One (1) Parking Space Deficit

The results from the shared parking analysis indicates that at full occupancy, the HBP parking structures would not be able to support the parking demand during the worst one AM hour of a Saturday of a typical 7-day week. A sensitivity assessment was conducted to determine possible mitigation measures that would allow for the proposed conversion to be approved. The analysis determined that the HBP parking structures can support the peak parking demand (at 10:00 AM on a Saturday) at full occupancy under the condition in which **1,486 SF of executive suite space** is prohibited from being occupied during weekend peak hours. Table 3-4 summarizes the findings of the analysis.

TABLE 3-4: Sensitivity Assessment
Future Condition: Adjusted Full Occupancy of HBP*

LAND USE	Existing-Occupied HBP Site		Existing-Vacant HBP Site		Hallway Use HBP Conversion to Office Use					TOTAL SHARED PARKING DEMAND
	Size	92,347 SF	24,236 SF	1,203 SF						
Unit Land Use	-	-	-	-	Visitor	Employee	Peak Total			PARKING DEMAND
Parking Ratio	-	-	-	-	0.3 /ksf	3.5/ksf	3.8/ ksf			
Gross Demand	433	65	0 spaces	5 spaces	5 spaces					
	Existing % of Peak	Total Number of Spaces	Existing % of Peak	Total Number of Spaces	% of Peak	# of Spaces	% of Peak	# of Spaces	Total # of Spaces	
7:00 AM	36.26%	157	20%	13	20%	0	20%	1	1	171
8:00 AM	63.74%	276	60%	39	60%	0	60%	3	3	318
9:00 AM	91.45%	396	80%	52	80%	0	80%	4	4	452
10:00 AM	100.00%	433	90%	58	90%	0	90%	5	5	496
11:00 AM	94.92%	411	100%	65	100%	0	100%	5	5	481
12:00 PM	87.53%	379	90%	58	90%	0	90%	5	5	442
1:00 PM	85.45%	370	80%	52	80%	0	80%	4	4	426
2:00 PM	76.67%	332	60%	39	60%	0	60%	3	3	374
3:00 PM	71.36%	309	40%	26	40%	0	40%	2	2	337
4:00 PM	62.59%	271	20%	13	20%	0	20%	1	1	285
5:00 PM	60.51%	262	10%	6	10%	0	10%	1	1	269
6:00 PM	56.12%	243	5%	3	5%	0	5%	0	0	246
7:00 PM	45.50%	197	0%	0	0%	0	0%	0	0	197

***Adjusted Full Occupancy of HBP is defined as the condition in which only 1,486 SF of the Executive Suite Spaces) out of the entire building are prohibited to be occupied during the peak weekend hour.**

The results shown in Table 3-4 reveal that the total shared parking demand under the condition stated above would result in 496 occupied parking spaces. Given the existing supply of 496 spaces, **the HBP parking structure will operate at capacity with no parking space deficits during peak weekend hours.**





4.0 CONCLUSIONS AND RECOMMENDATIONS

Minagar & Associates, Inc. conducted a comprehensive parking study for the Hermosa Beach Pavilion (HBP) with respect to the proposed conversion of 1,203 SF first floor spec suites (office or retail). The parking study included an analysis of hourly parking utilization data over a 21-day period from January 15, 2025 to February 5, 2025, and an assessment of relevant City Code parking requirements. Based upon the composite results of these analyses, the following conclusions were derived:

1. The existing, attached six-level enclosed parking structure provides the HBP with a total of 496 available parking spaces. During the peak weekend, a maximum of 433 of these parking spaces are occupied during the peak hour (87.29% maximum utilization), leaving 63 vacant parking spaces remaining under the existing conditions. Peak parking activity at the HBP site was observed to occur typically around 10:00 a.m. on weekends.
2. Using the City of Hermosa Beach's Municipal Code "parking rates" to directly determine off-street parking space requirements, the existing HBP site is required to provide 686 spaces to accommodate the parking of its existing tenant land uses.
3. The difference of 253 parking spaces between the City's 686-space requirement and the actual surveyed maximum accumulation of 433 parked vehicles is attributable to the prevailing shared parking activities observed at the HBP. At its most fundamental level, shared parking is demonstrated when one parking space is used to serve two or more land uses, and is the result of motorists using a single auto trip and parking space to visit multiple land uses.
4. The proposed interior developments include the 1203 SF conversion of first floor common area to office/retail use spec suites at HBP. City Code-based parking requirements for the HBP site under this condition are represented by the baseline peak parking demand from the parking utilization data (433 spaces), plus the theoretical parking demand from the vacant suites (96 spaces) plus the calculated parking demand of the proposed conversion (5 spaces) through the direct application of the City of Hermosa Beach's parking ratio formulas (1 space per 250 SF for office or retail use). This results in a total parking requirement of





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534 spaces. Given the existing supply of 496 spaces, this yields a theoretical deficit of 38 spaces.

6. Consistent with the ITE/ULI shared parking methodology, Minagar & Associates, Inc. conducted a shared parking analysis to more realistically estimate the parking demand requirements of the HBP under the proposed conditions at full occupancy. Shared parking demand projections for the "proposed" condition (i.e., HBP conversion of 1,203 SF first floor spec suites to office/retail use) reveals the peak shared-parking demand for the HBP to be 497 parking spaces.

7. **Given the existing parking supply of 496 parking spaces within the attached parking structure, it is determined that the HBP does provide an adequate supply of off-street parking spaces to accommodate the proposed conversion of 1,203 SF of hallway space to office or retail use contingent upon the condition in which 1,486 SF of executive office space is not occupied during peak weekend hours.**





FINAL PARKING STUDY

HERMOSA BEACH PAVILION AT 1601 PACIFIC COAST HIGHWAY
CONVERSION OF 1,203 SF OF HALLWAY SPACE USE TO OFFICE/RETAIL USE

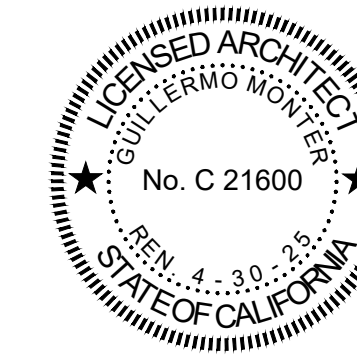
FEBRUARY 19, 2025

Appendix A

Proposed Hermosa Beach Pavilion Site Plans



SEALS & SIGNATURES



PROJECT NAME & LOCATION:

1601 PCH

STREET LEVEL
SPEC SUITES

1601 PACIFIC COAST
HIGHWAY
HERMOSA BEACH, CA

ISSUED FOR PLAN CHECK

1601 PCH

STREET LEVEL SPEC SUITES

1601 PACIFIC COAST HIGHWAY

HERMOSA BEACH, CA

DRAWING INDEX

- INTERIOR DESIGN:**
- A0-00 COVER SHEET
 - A0-01 GENERAL NOTES
 - A0-02 GENERAL NOTES
 - A0-03 GENERAL NOTES
 - A0-04 GENERAL NOTES
- PARKING STRUCTURE PLANS**
- A2.1 AS-BUILT PARKING P2 LEVEL FLOOR PLAN
 - A2.2 AS-BUILT PARKING P3 LEVEL FLOOR PLAN
 - A2.3 P4 LEVEL FLOOR PLAN
 - A2.4 P5 LEVEL FLOOR PLAN
 - A2.5 AS-BUILT PARKING P6 LEVEL FLOOR PLAN
- INTERIOR DESIGN**
- A1-00 DEMOLITION PLAN
 - A1-01 PARTITION PLAN
 - A3-00 DEMOLITION REFLECTED CEILING PLAN
 - A3-01 REFLECTED CEILING PLAN
 - A4-01 FINISH PLAN
 - A5-01 EGRESS PLAN
 - A6-01 ELEVATIONS
 - A7-01 DETAILS
 - A7-03 GLAZING DETAILS
 - DA-01 PATH OF TRAVEL
 - DA-02 EXISTING ACCESSIBLE RESTROOMS
 - DA-03 ACCESSIBLE NOTES
 - DA-04 ACCESSIBLE NOTES

AMERICAN WITH DISABILITIES ACT (ADA) ARE FEDERAL OBLIGATIONS ONLY AND IS NOT REVIEWED BY LOCAL ENFORCEMENT AGENCIES. THIS PROJECT SHALL COMPLY WITH THE DISABLED ACCESS PROVISIONS SET FORTH IN THE CURRENT CALIFORNIA BUILDING CODE (CBC). IT IS THE PROPERTY OWNER'S AND THEIR AGENT'S RESPONSIBILITY TO COMPLY WITH ALL FEDERAL, STATE AND LOCAL CODES.

ABBREVIATIONS

ACCES ACCESSORY	EMBED EMBED(D)(ING)	KIT KITCHEN	REIN REINFORCE(D)(ING)(MENT)
ACOUS ACOUSTICAL)	ENGR ENGINEER(ED)	LAV LAVATORY	RESIL RESILIENT
AFF ABOVE FINISHED FLOOR	ENTR ENTRANCE	LB POUND	RFG ROOFING
AL ALUMINUM	EQ EQUIPMENT	LT LIGHT	RM ROOM
ALT ALTERNATE	EXP EQUIPMENT	LVLG LEVELING	RO ROUGH OPENING
ANNUNC ANNUNCIATOR	EXIST EXISTING	LVR LOUVER	SCR SCRIBE
ANOD ANODIZED	EXP JT EXPANSION JOINT	MAX MAXIMUM	SECUR SECURITY
APPL APPLIANCE	EXPS EXPOSE(D)	MFD MANUFACTURED	SF SQUARE FEET
ARCH ARCHITECT(URAL)	EXT EXTERIOR	MFR MANUFACTURER	SGL SINGLE
AUTO AUTOMATIC	FAB FABRICATION	MECH MECHANICAL	SHORG SHORING
AVG AVERAGE	FD FLOOR DRAIN	MET METAL	SIM SIMILAR
& AND	FE FIRE EXTINGUISHER	MEMB MEMBRANE	SST STAINLESS STEEL
BLDG BUILDING	FEC FIRE EXTINGUISHER CABINET	MEZZ MEZZANINE	STD STANDARD
BOLL BOLLARD	FHC FIRE HOSE CABINET	MINI MINIMUM	STL STEEL
BD BOARD	FIN FINISH	MISC MISCELLANEOUS	STRFR STOREFRONT
BLKG BLOCKING	FLDG FOLDING	MLWK MILLWORK	STRUCT STRUCTURAL
BRODM BROADLOOM	FPLC FIREPLACE	MOIST MOISTURE	SURF SURFACE
BU BUILT UP	FR FIRE RAT(ING)(ED)	MOT MOTOR(IZED)	SUSP SUSPENDED
CAB CABINET	FRMG FRAMING	MTD MOUNTED	SYS SYSTEM(S)
CPT CARPET	FXD FIXED	NIC NOT IN CONTRACT	THK THICK
CEM CEMENT(ITIOUS)	FXTR FIXTURE	NO NUMBER	TLT TOILET
CER CERAMIC	FLR FLOOR(ING)	NTS NOT TO SCALE	TRAF TRAFFIC
CLG CEILING	FURN FURNITURE	ORNA ORNAMENTAL	TRANS TRANSPARENT
COATG COATING	FWC FABRIC WALL COVERING	OVFL OVERFLOW	TRTD TREATED
COILG COILING	GA GAUGE	OVHD OVERHEAD	T&G TONGUE AND GROOVE
CONC CONCRETE	GFRG GLASS FIBER REINFORCED CONCRETE	OPNG OPENING(S)	TYP TYPICAL
CONSTR CONSTRUCTION	GFRG GLASS FIBER REINFORCED CONCRETE	OPR OPERABLE	UNDRLAY UNDERLAYMENT
CONT CONTINUOUS(ATION)	GFRG GLASS FIBER REINFORCED GYPSUM	PTR PARTITION	UTIL UTILITY
CONTR CONTRACT(OR)	GFRP GLASS FIBER REINFORCED PLASTER	PEDTR PEDESTRIAN	U.N.O. UNIFORM OBJECT
COV COVER	GL GRAD(E)(ING)	PBD PARTICLE BOARD	VEH VEHICLE
CMU CONCRETE MASONRY UNIT	GYP GYPSUM	PNL PANEL	VERT VERTICAL
DBL DOUBLE	HD HDWD	POLYST POLYSTYRENE	VIF VERIFY IN FIELD
DEPT DEPARTMENT	GR GRAD(E)(ING)	PORT PORTABLE	W/ WITH
DES DESIGN(ED)	GYP GYPSUM	PREFIN PREFINISHED	WC WATER CLOSET
DET DETAIL	HD HDWD	PREFAB PREFABRICATED	WO WOOD
DF DRINKING FOUNTAIN	HDWD HARDWOOD	PLAM PLASTIC LAMINATE	WDW WINDOW
DIA DIAMETER	HDWE HARDWARE	PLAS PLASTER	W/O WINDOW NOTED OTHERWISE
DIF DIFFUSER	HM HOLLOW METAL	PLSTC PLASTIC	WT WEIGHT
DIM DIMENSION	HORIZ HORIZONTAL	PLYWD PLYWOOD	WTRPRF WATERPROOFING
DISP DISPENSER	HVAC HEATING, VENTILATING, AND AIR CONDITIONING	PRTECN PROTECTION	
DIV DIVISION	INFO INFORMATION	RDR READER	
DN DOWN	INSTRUM INSTRUMENT(ATION)	RECES RECESSED	
\$ DOLLAR (US CURRENCY)	INSUL INSULATION	RECEPT RECEPTACLE	
DR DOOR	INTLK INTERLOCK(ING)	REF REFERENCE	
DSCON DISCONNECT	INT INTERIOR	REFL REFLECTED	
DWR DRAWER	INFILTR INFILTRATION	REFR REFRIGERATOR	
ELAST ELASTOMERIC	JAN JANITOR	REQD REQUIRED	
ELEC ELECTRICAL		RESIS RESISTANT(IVE)	

PROJECT SUMMARY

THE PROJECT CONSISTS OF TENANT IMPROVEMENT

DESCRIPTION OF ALTERATIONS WORK:
CONSOLIDATION OF 4 EXISTING RETAIL TENANT SITES AND PORTION OF EXISTING ARCADE COMMON SPACE INTO 2 SPEC OFFICE SUITES
NEW PARTITIONS AND ELECTRICAL

EXISTING SF FOR EACH SUITE:
SUITE #155 - 833 SF
SUITE #160 - 663 SF
SUITE #165 - 1,173 SF
SUITE #170 - 2,623 SF

EXISTING COMMON SPACE: 4,275 SF

AMOUNT OF SF BEING ADDED TO EACH EXISTING SUITE:
SUITE #155 - 811 SF
SUITE #165 - 392 SF

NEW TOTAL SF FOR EACH SUITE:
SUITE #155 - 2,254 SF
SUITE #165 - 4,317 SF

FIRE SUPPRESSION SPRINKLERS: ALTER EXISTING SYSTEM AND ADD NEW CONSTRUCTION, KEEPING EXISTING IN OPERATION.
FIRE ALARM: ALTER EXISTING SYSTEM AND ADD NEW CONSTRUCTION, KEEPING EXISTING IN OPERATION.
TELEPHONE: ALTER EXISTING SYSTEM AND ADD NEW CONSTRUCTION, KEEPING EXISTING IN OPERATION.

DESIGN-BUILD NOTES:

- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DRAWINGS, CALCULATIONS, GOVERNMENTAL AGENCY APPROVALS AND FEES TO COMPLETE THIS WORK FOR ALL SPRINKLER AND FIRE/LIFE SAFETY WORK/SCOPE.
- ARCHITECTURAL DRAWINGS SUPERSEDE ALL ENGINEERING DRAWINGS FOR QUANTITY AND LOCATION OF MATERIALS AND FIXTURES.
- THE GENERAL CONTRACTOR AND/OR THEIR SUBCONTRACTORS ARE RESPONSIBLE FOR ANY REQUIRED CERTIFIED CALCULATIONS AND/OR DRAWINGS REQUIRED TO SATISFY INSPECTION REQUIREMENTS THAT RESULT FROM ON-SITE CONDITIONS OR WORK PERFORMED.

SEISMIC GAS VALVE:

AN APPROVED SEISMIC GAS SHUTOFF VALVE SHALL BE INSTALLED ON THE FUEL GAS LINE ON THE DOWN STREAM SIDE OF THE UTILITY METER AND BE RIGIDLY CONNECTED TO THE EXTERIOR OF THE BUILDING OR STRUCTURE CONTAINING THE FUEL GAS PIPING. PER ORDINANCE 170.158) (INCLUDES COMMERCIAL ADDITIONS AND TI WORK OVER \$10,000) SEPARATE PLUMBING PERMIT IS REQUIRED.

CONSTRUCTION IN RIGHT-OF-WAY:

THE CONSTRUCTION SHALL NOT RESTRICT A FIVE-FOOT CLEAR AND UNOBSTRUCTED ACCESS TO ANY WATER OR POWER DISTRIBUTION FACILITIES (POWER POLES, PULL-BOXES, TRANSFORMERS, VAULTS, PUMPS, VALVES, METERS, APPURTENANCES, ETC) OR TO THE LOCATION OF THE HOOK-UP. THE CONSTRUCTION SHALL NOT BE WITHIN TEN FEET OF ANY POWER LINES - WHETHER OR NOT THE LINES ARE LOCATED ON THE PROPERTY. FAILURE TO COMPLY MAY CAUSE CONSTRUCTION DELAYS AND / OR ADDITIONAL EXPENSE.

APPLICABLE CODES

- APPLICABLE CODES**
- 2022 CBC (BASED ON 2021 IBC)
 - 2022 CPC (BASED ON 2021 UPC)
 - 2022 CEC (BASED ON 2021 NEC)
 - 2022 CMC (BASED ON 2021 UMC)
 - 2022 CBC ENERGY CODE TITLE 24, PART 6
 - 2022 CGBC
 - *2023 CITY OF HERMOSA BEACH, CA AMENDMENTS PER ORDINANCE 16-425
 - 2019 CALIFORNIA FIRE CODE

CHAPTER 3 USE AND OCCUPANCY CLASSIFICATION

CLASSIFICATION

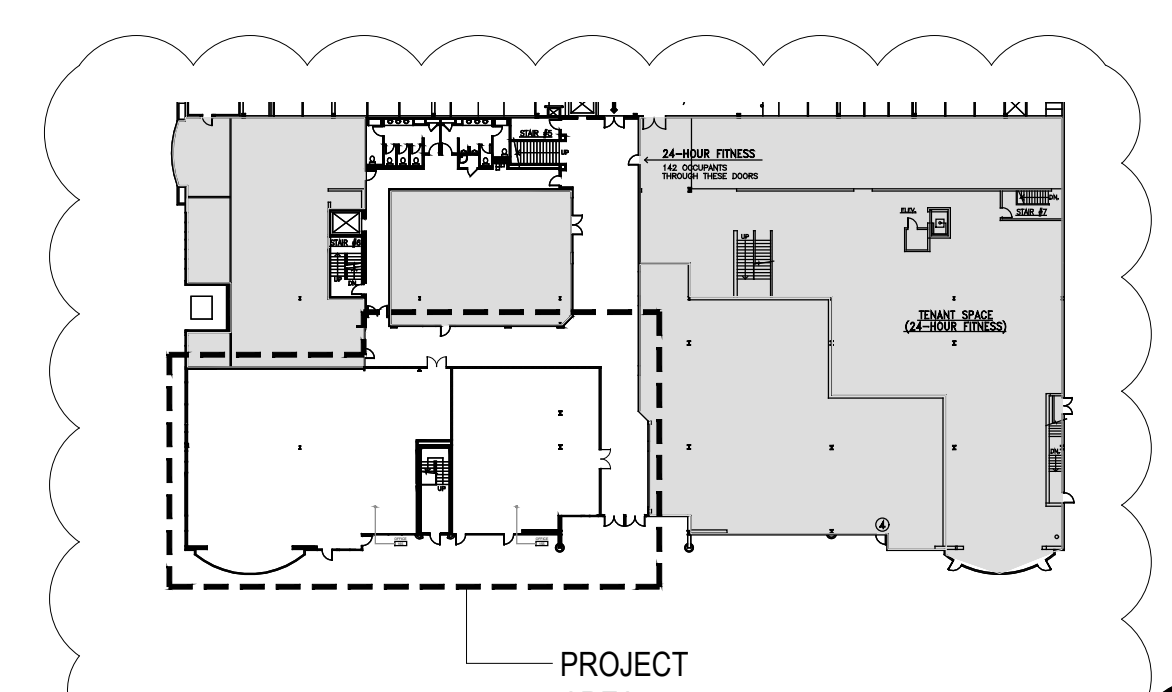
- GROUP B: BUSINESS

TOTAL USABLE SPACE: 6,571 USF
TOTAL AREA OF WORK: 6,571 USF
ACCESSIBLE RESTROOMS: YES
NUMBER OF STORIES: 2
CONSTRUCTION TYPE: TYPE 1-FR
SPRINKLERED BUILDING: YES

DEFERRED SUBMITTALS:

FIRE SPRINKLERS, FIRE ALARM, LOW VOLTAGE
MEP UNDER SEPARATE PERMITS

KEY PLAN



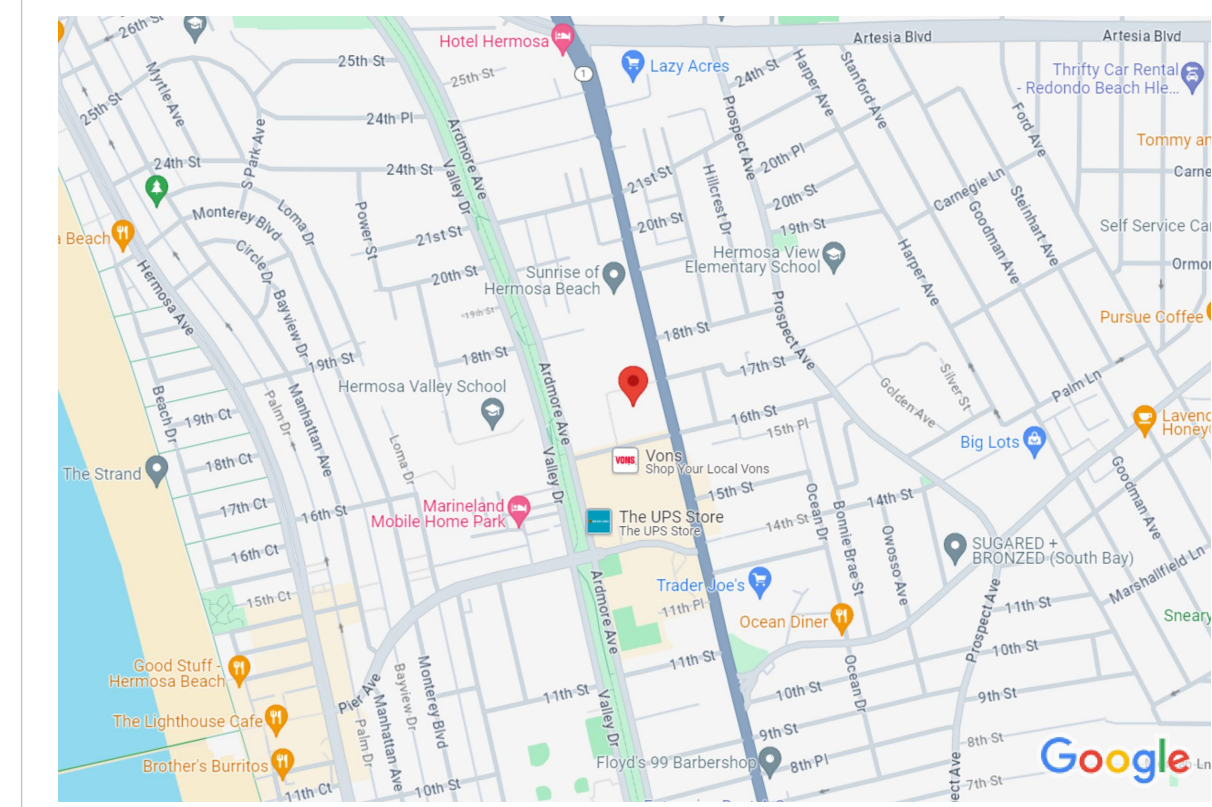
PROJECT DIRECTORY

BUILDING MANAGEMENT:
CBRE, INC.

INTERIOR DESIGNER
ONE design collective
TEL: (562) 355-0879
CONTACT: SHANNON MCSHEA

BUILDING OWNER
1601 PCH LP
C/O HIDDEN HARBOR CAPITAL MANAGEMENT
1601 PACIFIC COAST HIGHWAY, SUITE 290
HERMOSA BEACH, CA 90254
TEL: (310) 612-3594

VICINITY MAP



PROJECT NO.: 1264

SCALE: NA

SHEET SIZE: 30x42

DRAWN BY: SRM

CHECKED BY: GM

SHEET TITLE:

COVER SHEET

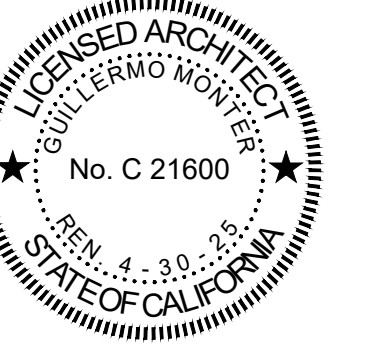
SHEET NUMBER:

A0-00



design collective
Telephone (562) 355-0879

SEALS & SIGNATURES



SIGNED ON JULY 26, 2024

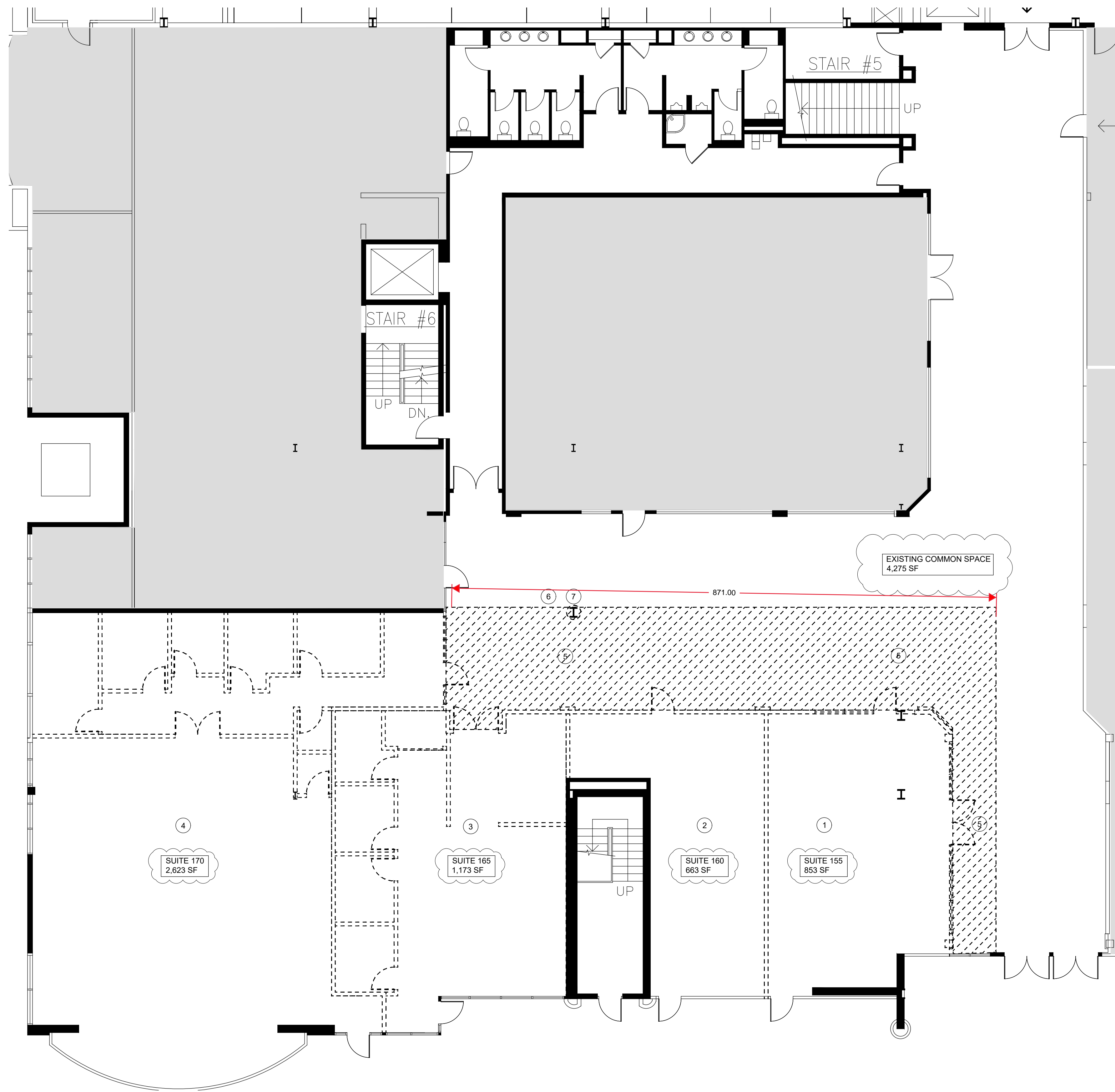
PROJECT NAME & LOCATION:

1601 PCH

STREET LEVEL
SPEC SUITES

1601 PACIFIC COAST
HIGHWAY
HERMOSA BEACH, CA

ISSUED FOR PLAN CHECK



DEMOLITION PLAN

SCALE
1/8" = 1'-0" 1

DEMOLITION NOTES

- REFER TO "A0.00", "A0.01", "A0.02", "A0.03" AND "A0.05" DRAWINGS FOR ADDITIONAL NOTES AND INFORMATION.
- SEE REFLECTED CEILING PLAN FOR ANY REQUIRED CEILING DEMOLITION.
- PATCH AND SKIM COAT WALLS TO RECEIVE SPECIFIED WALL FINISH.
- SAVE DOORS, FRAMES AND HARDWARE FOR RELOCATION OR RETURN TO BUILDING STOCK. COORDINATE STORAGE WITH BUILDING MANAGEMENT.
- EXISTING WINDOW COVERINGS TO BE REMOVED, CLEANED AND REINSTALLED AFTER COMPLETION OF CONSTRUCTION. CHECK BUILDING STOCK BEFORE GENERATING NEW ORDER FOR MISSING ITEMS.
- REMOVE ALL EXISTING FLOORING FINISHES (CARPET & PAD) AND BASE. CONFIRM REMOVAL OF HARD SURFACE FLOORING, WOOD, TILE, VCT & STONE WITH ONE design collective. PREP DECK FOR NEW SCHEDULED FINISH.

DEMOLITION KEY NOTES

- SUITE 155
DEMO EXISTING STOREFRONT THROUGHOUT.
DEMO ALL WALLS, DOORS/FRAMES, ALL MILLWORK/DISPLAY CABINETS.
DEMO ALL EXISTING CEILING, SOFFITS, LIGHTING AND HVAC.
DEMO ALL KITCHEN EQUIPMENT AND PLUMBING THROUGHOUT.
DEMO ALL FLOORING.
- SUITE 160
DEMO EXISTING STOREFRONT THROUGHOUT.
DEMO ALL WALLS, DOORS/FRAMES AND ANY MILLWORK.
DEMO ALL EXISTING CEILING, SOFFITS, LIGHTING AND HVAC.
DEMO ALL FLOORING.
- SUITE 165
DEMO EXISTING STOREFRONT THROUGHOUT.
DEMO ALL WALLS, MIRRORS, DOORS TYP.
DEMO ALL EXISTING CEILING, SOFFITS, LIGHTING AND HVAC.
DEMO ALL EXISTING MILLWORK AND PLUMBING THROUGHOUT.
DEMO ALL FLOORING.
- SUITE 170
DEMO EXISTING STOREFRONT THROUGHOUT.
DEMO ALL WALLS, MIRRORS, DOORS TYP.
DEMO ALL EXISTING CEILING, SOFFITS, LIGHTING AND HVAC.
DEMO ALL EXISTING MILLWORK AND RESTROOM PLUMBING THROUGHOUT.
DEMO ALL FLOORING, SPORTS MATS.
- DEMO EXISTING TERRAZZO TILE FLOOR IN HATCHED AREA ONLY.
TRY TO SALVAGE TILES IN GOOD CONDITION FOR POSSIBLE REUSE/FILL IN AFTER REMODEL.
- DEMO FAUX TREE TRUNKS IN ARCADE.
- DEMO FAUX TREE TRUNK - COLUMN TO REMAIN.

PARTITIONS

- EXISTING PARTITIONS TO BE DEMOLISHED
- EXISTING PARTITIONS TO REMAIN
- EXISTING DOOR TO BE DEMOLISHED

NUMBER	DESCRIPTION	DATE
3	ISSUE FOR PLAN CHECK	07/26/24
2	ISSUE FOR PRICING	03/06/24
1	ISSUE FOR CLIENT REVIEW	01/05/24

PROJECT NO.: 1264

SCALE: 1/8"=1'-0"

SHEET SIZE: 30X42

DRAWN BY: SRM

CHECKED BY: GM

SHEET TITLE:

DEMOLITION PLAN

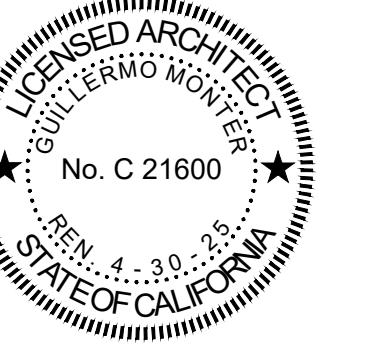
SHEET NUMBER:

A1-00



design collective
Telephone (562) 355-0879

SEALS & SIGNATURES



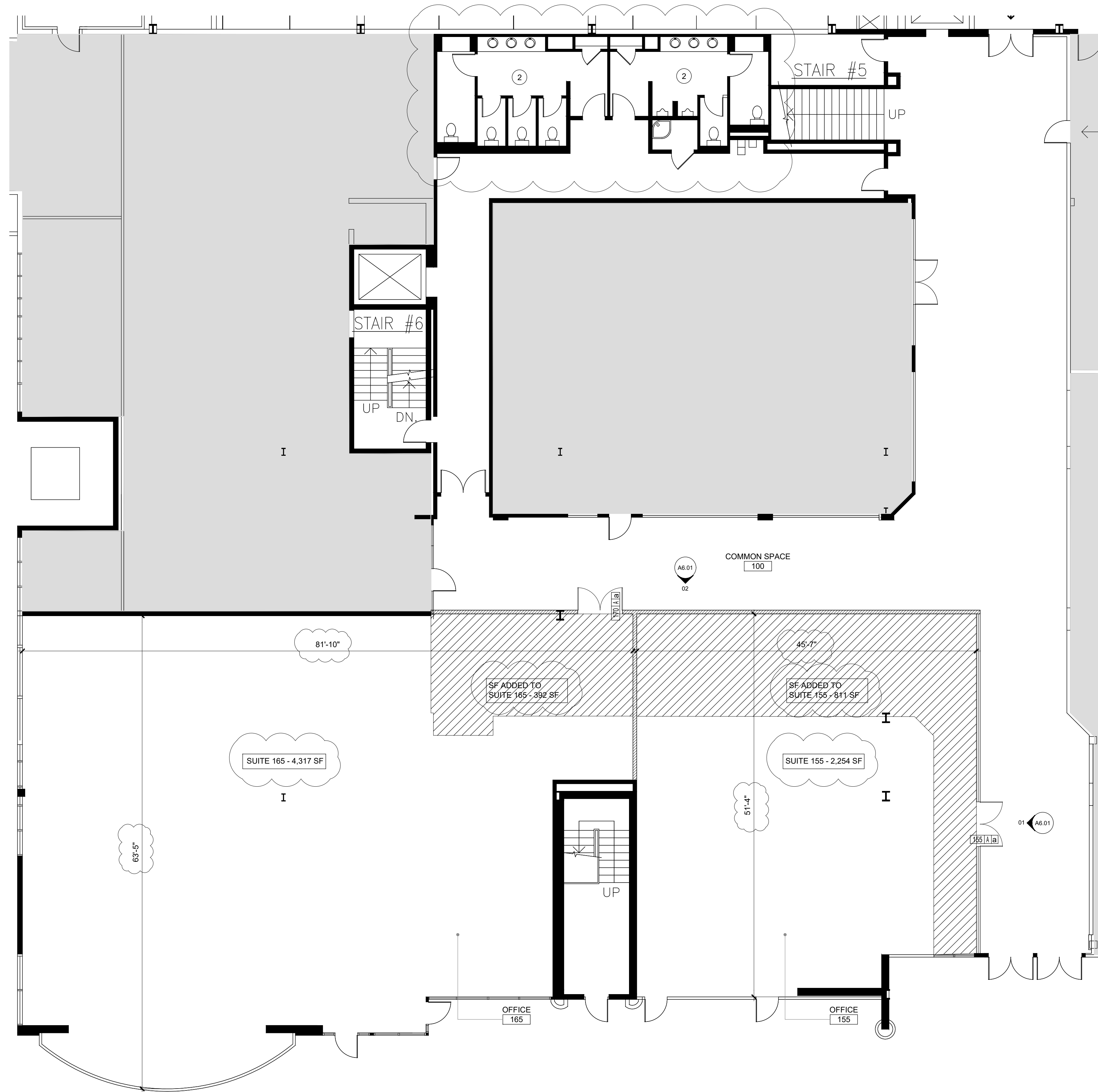
PROJECT NAME & LOCATION:

1601 PCH

STREET LEVEL
SPEC SUITES

1601 PACIFIC COAST
HIGHWAY
HERMOSA BEACH, CA

ISSUED FOR PLAN CHECK



PARTITION PLAN

PARTITION NOTES

- REFER TO 'A0.00', 'A0.01', 'A0.02', 'A0.03', 'A0.04' AND 'A0.05' DRAWINGS FOR ADDITIONAL NOTES AND INFORMATION.
- ALL TRADES TO INSPECT EXISTING SITE CONDITIONS PRIOR TO SUBMITTING BIDS. CONTRACTOR TO NOTIFY DESIGNER OF ANY CONFLICTS WITH THE WORK.
- REPAIR AND PATCH ALL SURFACES WHERE WALLS HAVE BEEN REMOVED OR SCARRED FROM DEMOLITION.
- REPAIR AND SKIM COAT ALL EXISTING PARTITIONS TO RECEIVE NEW FINISHES.
- CONTRACTOR TO PREPARE ALL FLOORING SURFACES TO RECEIVE NEW FINISHES. FLASH PATCH AS REQUIRED FOR SMOOTH FINISH. PATCH ALL HOLES IN THE SLAB TO MATCH EXISTING CONDITION.
- CONTRACTOR TO REVIEW PARTITION TYPES AGAINST MECHANICAL DRAWINGS TO COORDINATE AREAS OF PLENUM AND NON-PLENUM CEILING.
- CONTRACTOR SHALL FURR OUT AND/OR REMOVE PORTIONS OF EXISTING WALL CONSTRUCTION AS REQUIRED TO INSTALL NEW OUTLETS, SWITCHES, CONDUITS, PIPING, THERMOSTATS, ETC. AND REFINISH AS REQUIRED TO RECEIVE NEW FINISH.
- EXTEND AND/OR MODIFY THE H.V.A.C., FIRE LIFE SAFETY & SPRINKLER SYSTEMS AS NEEDED TO ACCOMMODATE CURRENT CONSTRUCTION AND REVISED PARTITION CONFIGURATIONS. REFER TO ENGINEERING DRAWINGS.
- REPLACE ALL WINDOW COVERINGS WHICH WERE REMOVED DURING DEMOLITION.
- PROVIDE NEW BUILDING WINDOW COVERINGS AT EXTERIOR WINDOWS WHERE REQUIRED BASED ON NEW LAYOUT.
- FIRE / LIFE SAFETY & SPRINKLER ENGINEERING SHALL BE DESIGN BUILD, AND G.C. MUST INCLUDE ENGINEERING COSTS IN THE BID. CONTACT BUILDING MANAGEMENT REGARDING LIST OF MANDATORY / SUGGESTED SUB-CONTRACTORS AND BUILDING STANDARDS.
- PATCH DRYWALL AT ALL ABANDONED OUTLET LOCATIONS.
- ALL LOCKSETS SHALL BE CODED AND/OR KEYPED IN ACCORDANCE WITH BUILDING REQUIREMENTS. CODES AND/OR KEYS ARE TO BE DELIVERED TO TENANT PROPERLY TESTED AND/OR TAGGED. THE NUMBER OF MASTER AND PASS KEYS SHALL BE COORDINATED WITH BUILDING MANAGEMENT UNLESS OTHERWISE NOTED.
- EGRESS DOORS SHALL BE READILY OPERABLE FROM THE EGRESS SIDE OF THE WITHOUT THE USE OF A KEY OR ANY SPECIAL KNOWLEDGE OR EFFORT.

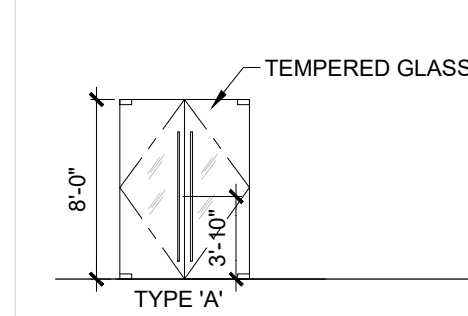
EQUIPMENT

- FIRE EXTINGUISHER CABINET USE LARSEN'S ARCHITECTURAL SERIES, PAINTED STEEL VERTICAL DUO PAINT TO MATCH PARTITION WITHOUT LOCK. SEMI-RECESSED 2409-R3, R.O. 10 1/2" W X 3" D X 25" H WITH 2A-10BC FIRE EXTINGUISHER WITH SFM TAG. VERIFY LOCATION AND COUNTS WITH FIRE INSPECTOR PRIOR TO CLOSING WALLS.

PARTITION KEYNOTES

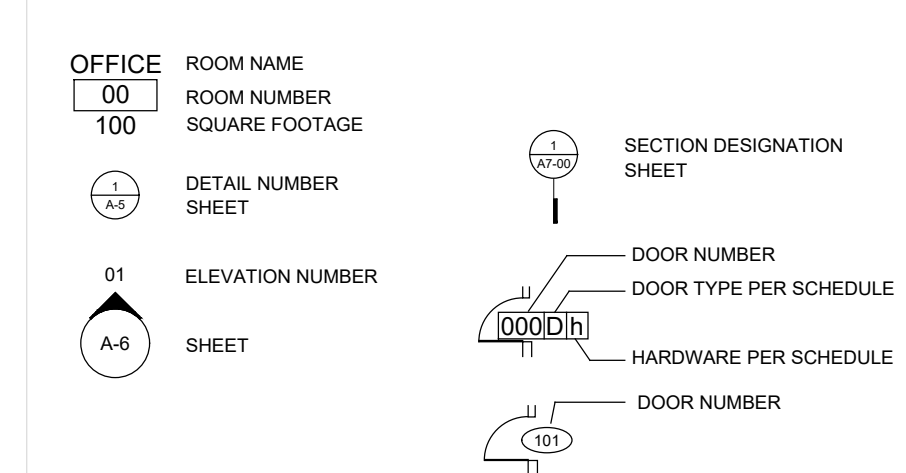
- SLAB TO SLAB PARTITION TO BE CONFIRMED WITH BUILDING ENGINEER. RATING CONFIRMATION REQUIRED.
- EXISTING RESTROOMS TO BE SHARED WITH THE ENTIRE BUILDING.

DOOR, FRAME AND HARDWARE TYPES



DOOR SCHEDULE	
DOOR FRAMES TO MATCH EXISTING WITH CONTINUOUS MITER U/L/O	
ASSEMBLIES MORE THAN 20 MINUTES OR DOUBLE EGRESS USE HOLLOW METAL WELDED FRAMES WITH 2" FRAME PROFILE PAINT/SELECT COLOR. WITH 8 SILENCERS.	
A	STRUCTURAL GLASS PAIR: 80" X 96" X 3/4" SATIN POLISHED STAINLESS STEEL. 4 FLOOR DOOR STOPS 2 HEADER MOUNTED STOPS. SEE DOOR ELEVATIONS FOR HANDLE LOCATIONS AND OR LAURENCE PATCH TYPES.
HARDWARE SCHEDULE	
ALL HARDWARE TO BE US26 (B26) FINISH - SATIN CHROME U.N.O.	
a	SUITE ENTRY PAIR LOCKSET: TO MATCH EXISTING
HARDWARE SPECIFICATIONS	
ALL HARDWARE TO BE SATIN CHROME FINISH UNO. ALL LEVERS TO BE L-03 WITH ESCUTCHEON UNO	
DEVICE	MANUFACTURER
CLOSER	LCN LCN 1461 SMOOTH SLIM LINE COVER US26D. INSTALL ON INTERIOR SIDE OF DOORS.
COORDINATOR	IVES COR MATCH DOOR FRAME FINISH WITH FL FILLER BARS AS REQUIRED.
AUTO FLUSH BOLTS	IVES FB41P WITH DUST PROOF STRIKE DP1 AT THRESHOLD, DP2 AT FLOOR MATCH HARDWARE FINISH.
THRESHOLD	PEMKO 1665A MILL FINISH ALUMINUM
HINGES	HAGER BB NRP 1279 BALL BEARING NON-REMOVABLE PIN 4-1/2" X 4-1/2"
	BB 1279 BALL BEARING 4-1/2" X 4-1/2" BB 1279
	1279 STANDARD 4-1/2" X 4-1/2"
	ETW ELECTRIC TRANSFER 4-1/2" X 4-1/2"
DOOR STOP	MOCKETT D53 CLEAR ANODIZED ALUMINUM
COAT HOOK	MOCKETT CH23 METALLIC SILVER (23)
	END

SYMBOLS



PARTITIONS

- EXISTING PARTITIONS TO REMAIN
- SLAB TO SLAB NON RATED PARTITION, SEE DETAIL
- 1 HOUR RATED PARTITION, SEE DETAIL
- GLASS PARTITION, SEE DETAIL

SCALE
1/8" = 1'-0"

1

NUMBER	DESCRIPTION	DATE
3	ISSUE FOR PLAN CHECK	07/26/24
2	ISSUE FOR PRICING	03/08/24
1	ISSUE FOR CLIENT REVIEW	01/05/24

PROJECT NO.: 1264

SCALE: 1/8"=1'-0"

SHEET SIZE: 30X42

DRAWN BY: SRM

CHECKED BY: GM

SHEET TITLE:

PARTITION PLAN

SHEET NUMBER:

A1-01



design collective
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SEALS & SIGNATURES

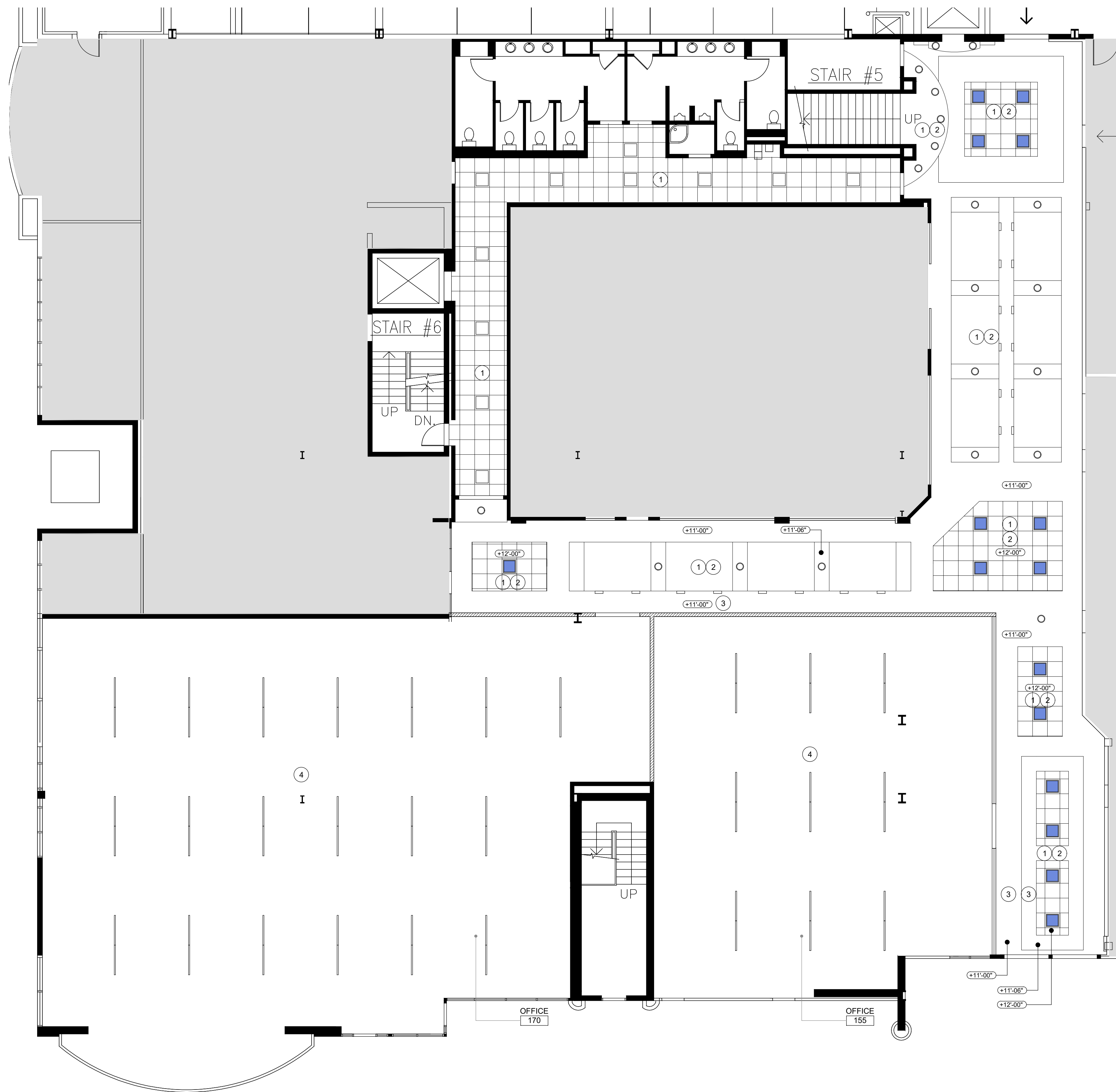
PROJECT NAME & LOCATION:

1601 PCH

STREET LEVEL
SPEC SUITES

1601 PACIFIC COAST
HIGHWAY
HERMOSA BEACH, CA

ISSUED FOR PRICING



CEILING PLAN

SCALE
1/8" = 1'-0" 1

REFLECTED CEILING NOTES

- SEE 'A0-00', 'A0-01', 'A0-02', 'A0-03', 'A0-04' AND 'A0-05' DRAWINGS FOR ADDITIONAL NOTES AND INFORMATION.
- ALL LIGHTING FIXTURES SHOWN ON PLAN ARE EXISTING UNLESS OTHERWISE NOTED.
- REFER TO ENGINEERING FOR NEW FIXTURES AND DAYLIGHT CONTROLS
- REFER TO MEP DRAWINGS FOR RELOCATION OR RECONFIGURATION OF EXISTING BUILDING RISERS, DUCTS OR PIPING IN RENOVATED AREAS.
- RE-WORK/ADD HVAC AS REQUIRED PER ENGINEERING DRAWINGS.
- CONTRACTOR TO RE-USE EXISTING HVAC AND REDISTRIBUTE AND ADD NEW AS REQUIRED FOR NEW LAYOUT, CODES AND BUILDING STANDARD. REFER TO MECHANICAL DRAWINGS
- ALL CEILING MOUNTED DEVICES, SENSORS, FIRE LIFE SAFETY ETC. TO BE WHITE.
- RE-WORK EXISTING FIRE/LIFE/SAFETY DEVICES AND PROVIDE NEW AS REQUIRED TO COMPLY WITH BUILDING STANDARD, LOCAL CODE AND NEW LAYOUT. THIS WORK SHALL BE FILED UNDER SEPARATE PERMIT.
- RE-WORK EXISTING SPRINKLER SYSTEM AS REQUIRED. THIS WORK TO BE FILED UNDER SEPARATE PERMIT. PROVIDE CONCEALED HEADS IN AREAS OF GYP. BOARD CONSTRUCTION.
- REPLACE MISSING OR DAMAGED/STAINED SPRINKLER COVERS THROUGHOUT SUITE.
- PROVIDE NEW TENANT STANDARD 2'X2' ACOUSTIC CEILING GRID AND TILES IN AREAS OF CONSTRUCTION/NEW CEILING. GC TO CONSOLIDATE EXISTING CEILING TILES WITH EXISTING AND NEW TILES WITH NEW.

REFLECTED CEILING KEY NOTES

- EXISTING LIGHTING TO REMAIN THROUGHOUT ARCADE.
- ALTERNATE - REPLACE ALL EXISTING LIGHTING IN ARCADE PER THE SPECIFICATIONS.
- MATCH NEW DRYWALL SOFFIT HEIGHT TO THE ADJACENT SOFFIT HEIGHT.
- EXPOSED CEILING THROUGHOUT NEW OFFICE SPEC SUITES.

REFLECTED CEILING PLAN SYMBOLS

- NEW ACOUSTICAL CEILING TILE TO MATCH EXISTING (GC TO CONFIRM SPEC)
- CEILING HEIGHT FROM FINISH FLOOR

REFLECTED CEILING PLAN SYMBOLS

MATCH EXISTING LAMP APPEARANCE CLASSIFICATION. IF NEW THROUGHOUT ALL LAMPS SHALL HAVE CRI OF MIN 80 + 3500K (835) CLASSIFICATION.

NEW	SPECIFICATION
A	HE WILLIAMS - AT2 LED ARCHITECTURAL TROFFER - CURVED DEEP CENTER
B	HE WILLIAMS - 4DR LED 4.5" DOWNLIGHT - ROUND
C	LED LIGHTING INSPIRE MID-IN COVE CHANNEL
D	HE WILLIAMS LED LLM ARCHITECTURAL SLIMLINE - EMERGENCY LIGHTING
	MATCH EXISTING

NUMBER	DESCRIPTION	DATE
1	ISSUE FOR PRICING	03/06/24
1	ISSUE FOR CLIENT REVIEW	01/05/24

PROJECT NO.: 1264

SCALE: 1/8"=1'-0"

SHEET SIZE: 30X42

DRAWN BY: SRM

CHECKED BY: GM

SHEET TITLE:

REFLECTED
CEILING PLAN

SHEET NUMBER:

A2-01

V:\PROJECTS\2024\1264 - 1601 PCH\240306\ARCHITECTURE\1601_PCH_A2-01_Ceiling_Plan.dwg



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SEALS & SIGNATURES

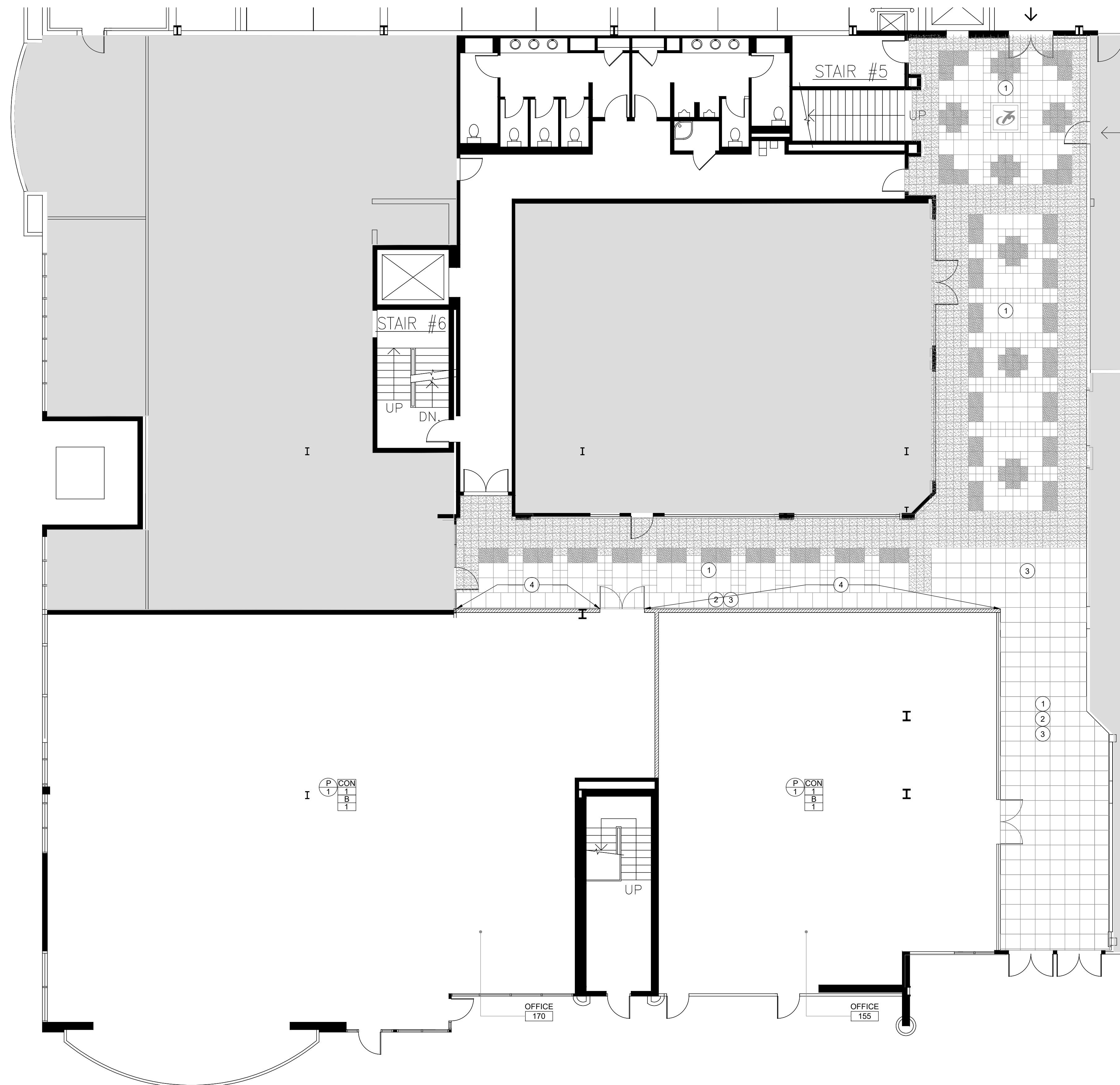
PROJECT NAME & LOCATION:

1601 PCH

STREET LEVEL
SPEC SUITES

1601 PACIFIC COAST
HIGHWAY
HERMOSA BEACH, CA

ISSUED FOR PRICING



FINISH PLAN

SCALE
1/8" = 1'-0" 1

FINISH NOTES

- SEE 'A0-00', 'A0-01', 'A0-02', 'A0-03', 'A0-04', AND 'A0-05' DRAWINGS FOR ADDITIONAL NOTES AND INFORMATION.
- ALL TRADES TO INSPECT EXISTING SITE CONDITIONS PRIOR TO SUBMITTING BIDS. CONTRACTOR TO NOTIFY DESIGNER OF ANY CONFLICTS WITH THE WORK.
- GC TO PROVIDE CERTIFIED INSTALLERS FOR ALL FLOORINGS.
- ALL EXTERIOR GLASS AND WINDOW MULLIONS TO BE CLEANED UPON COMPLETION OF CONSTRUCTION AS A PART OF THE GENERAL CONSTRUCTION CLEAN-UP.
- GC TO VERIFY EXISTING WINDOW SHADES MAY BE RE-INSTALLED (CORRECT SIZES) AT AREAS OF NEW OFFICE CONSTRUCTION. GC TO PROVIDE NEW IF REQUIRED.

FINISH KEY NOTES:

- EXISTING FLOORING AND FINISHES TO REMAIN THROUGHOUT ARCADE.
- IN ARCADE - GC TO INFILL WITH EXISTING, SALVAGED FLOOR TILE IF POSSIBLE.
- ALTERNATE - GC TO MATCH EXISTING FIELD TILE FOR NEW FLOOR LAYOUT IN THIS AREA ONLY.
- GC TO MATCH ARCADE PAINT COLOR AND APPLY AT NEW CONSTRUCTED WALLS.

FINISH SCHEDULE						
NO.	MATERIAL	MFG.	MFG. NO.	COLOR	REMARKS	CONTACT
WALL FINISHES						
P-1	PAINT	DUAN EDWARDS	DEWB3	COOL DECORBER	ALL SURFACES UN	NON-ACRYLIC GLOSS UNITS NOT TO EXCEED 18 FOR WALL. REMOVE 1/8" OF UNDESIRABLE EXISTING FINISHES. CLEANED AND FINISHED APPLIED TO INTERIOR WALLS AND CEILING. COATED WITH THE 100 GLOSS FINISH ESTABLISHED AT 1601 PCH. 1. APPLY TO ALL INTERIOR WALLS. 2. MAKE COORDINATION WITH ARCHITECT.
P-2	PAINT	DUAN EDWARDS	MATCH EXISTING	MATCH EXISTING	CEILING PAINT	NON-ACRYLIC GLOSS UNITS NOT TO EXCEED 18 FOR WALL. REMOVE 1/8" OF UNDESIRABLE EXISTING FINISHES. CLEANED AND FINISHED APPLIED TO INTERIOR WALLS AND CEILING. COATED WITH THE 100 GLOSS FINISH ESTABLISHED AT 1601 PCH. 1. APPLY TO ALL INTERIOR WALLS. 2. MAKE COORDINATION WITH ARCHITECT.
BASE						
B-1	1" RUBBER BASE	JOHNSONITE	BR	ROCK	ASTM E 846 METAL SHEET, OPTICAL RADIANT FLUX - CLASS 1. REMOVE 1/8" OF UNDESIRABLE EXISTING FINISHES. CLEANED AND FINISHED APPLIED TO INTERIOR WALLS AND CEILING. COATED WITH THE 100 GLOSS FINISH ESTABLISHED AT 1601 PCH. 1. APPLY TO ALL INTERIOR WALLS. 2. MAKE COORDINATION WITH ARCHITECT.	
FLOORING						
CON-1	CONCRETE			HAIRS SCRAPED		
TS-1	RUBBER TRANSITION	WYMP		TSB	CARPET TO CONCRETE TRANSITION	

NUMBER	DESCRIPTION	DATE
1	ISSUE FOR PRICING	03/06/24
1	ISSUE FOR CLIENT REVIEW	01/05/24

PROJECT NO.: 1264

SCALE: 1/8"=1'-0"

SHEET SIZE: 30X42

DRAWN BY: SRM

CHECKED BY: GM

SHEET TITLE:

FINISH
PLAN

SHEET NUMBER:

A4-01



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SEALS & SIGNATURES

PROJECT NAME & LOCATION:

1601 PCH

STREET LEVEL
SPEC SUITES

1601 PACIFIC COAST
HIGHWAY
HERMOSA BEACH, CA

SCALE
3/8" = 1'-0" 4

SCALE
3/8" = 1'-0" 3

SCALE
3/8" = 1'-0" 2

SCALE
3/8" = 1'-0" 1

ISSUED FOR PRICING

NUMBER	DESCRIPTION	DATE
1	ISSUE FOR PRICING	03/09/24
1	ISSUE FOR CLIENT REVIEW	01/05/24

ISSUED:

PROJECT NO.: 1264

SCALE: AS NOTED

SHEET SIZE: 30X42

DRAWN BY: SRM

CHECKED BY: GM

SHEET TITLE:

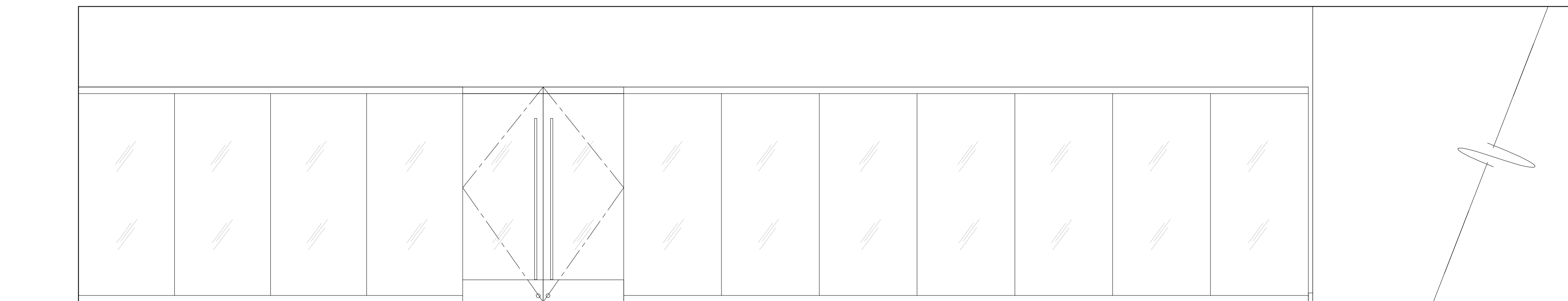
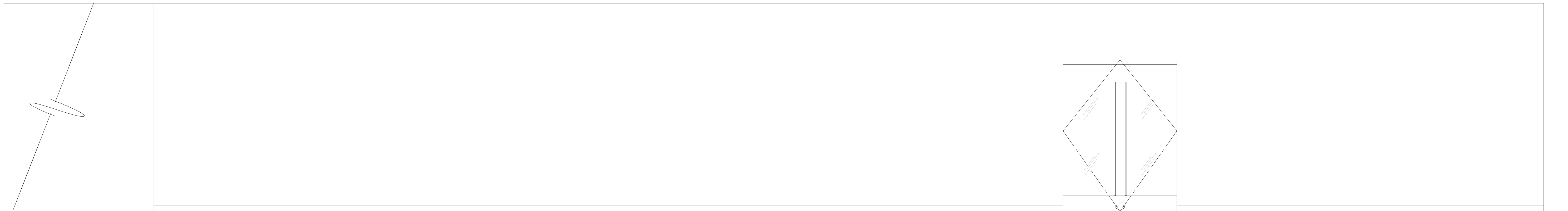
ELEVATIONS

SHEET NUMBER:

A6-01

EAST ELEVATION

SOUTH ELEVATION





FINAL PARKING STUDY

HERMOSA BEACH PAVILION AT 1601 PACIFIC COAST HIGHWAY
CONVERSION OF 1,203 SF OF HALLWAY SPACE USE TO OFFICE/RETAIL USE

FEBRUARY 19, 2025

Appendix B

City of Hermosa Beach's Municipal Code

(as of October 2021)





HERMOSA BEACH, CA MUNICIPAL CODE

TITLE 17 ZONING

Chapter 17.44 Off-Street Parking

Contents:

- 17.44.010 Definitions.
- 17.44.015 Applicability.
- 17.44.020 Off-street parking—Residential uses.
- 17.44.030 Off-street parking—Commercial and business uses.
- 17.44.040 Parking requirements for the downtown district.
- 17.44.050 Unlawful to reduce available parking.
- 17.44.060 Common parking facilities.
- 17.44.070 Off-street parking—Mixed uses.
- 17.44.080 Uses not otherwise specified.
- 17.44.090 Off-street parking location.
- 17.44.100 Size of spaces.
- 17.44.110 Tandem parking and entry-way standards.
- 17.44.120 Driveways.
- 17.44.130 Turning radii, stall width and aisle width.
- [Repealed] 17.44.140 Requirements for new construction.
- 17.44.150 Underground parking facilities.
- 17.44.160 Required improvement and maintenance of parking area.
- 17.44.170 Parking area in R-3 or R-P zones.
- 17.44.180 Resulting fractions.
- 17.44.190 Valet Parking.
- 17.44.200 Assignment of off-street residential parking spaces.
- 17.44.210 Parking plans.
- 17.44.220 Consolidated off-street parking.
- 17.44.230 Parking for reduced parking demand housing.
- 17.44.240 Mechanical vehicle lifts.

17.44.010 Definitions.

As used in this chapter:

Entrance-way means an opening or passageway to a building or structure which permits pedestrian or vehicular access to such building or structure.

Gross floor area means the total area occupied by a building or structure, excepting therefrom only the area of any inner open courts, corridors, open balconies (except when utilized, e.g., restaurant seating or similar usage), and open stairways. Such total area shall be calculated by measuring along the outside dimensions of the exterior surfaces of such building or structure.

Major city street means all public rights-of-way designated in the circulation element of the general plan as a primary, or secondary arterials or as collectors.

Mechanical vehicle lift means a mechanical system that lifts or descends one (1) vehicle to make space available to park a second vehicle in a vertical tandem fashion.

Off-street parking means parking upon private property as accessory to other permitted land uses, and shall not include publicly owned parking.

Tandem parking means one (10) automobile parked after or behind another in a lengthwise fashion. In this title, tandem parking is limited to not more than one (1) automobile behind another.

Underground parking facilities means a basement equipped, designed, used or intended to be used for parking automobiles. (Ord. 20-1419 §4(1), 2020; prior code Appx. A, § 1150)

17.44.015 Applicability.

The requirements of this Chapter apply to the establishment, alteration, expansion, or change in any use or structure, as provided below.

- A. New Buildings and Land Uses. On-site parking shall be provided in accordance with this Chapter at the time any main building or structure is erected or any new land use is established, except as provided below.
- B. Change of Use of Existing Buildings. When a new use is established in an existing building, parking shall be provided as follows:
 - 1. Late Night Alcohol Establishment. Where the new use is a late night serving alcohol establishment, parking in accordance with this Chapter shall be provided.
 - 2. Uses Other than Late Night Alcohol Establishment. When a new use other than a late night alcohol establishment is established in an existing building no additional parking spaces shall be required. Any addition or expansion to the existing building shall provide parking pursuant to 17.44.015. D, Additions or Expansions of Existing Buildings.
- C. Downtown and C-1 Districts.
 - 1. In the Downtown and C-1 Districts, no on-site parking is required for the first 5,000 square feet of ground floor non-residential, non-office, and non-late night alcohol establishment use. Parking in accordance with this Chapter shall be provided for ground floor area in excess of 5,000 square feet and for all upper story floor area.
 - 2. In the Downtown and C-1 Districts, residential, office, and late night alcohol establishments shall provide parking in accordance with this Chapter.
- D. Additions or Expansions of Existing Buildings.
 - 1. Additions less than 10 percent or 500 square feet. No additional parking is required for alterations to existing buildings that do not change the building square footage on site by more than 10 percent or 500 square feet, whichever is greater, provided the use is other than a late night alcohol establishment.

2. Additions greater than 10 percent or 500 square feet. Additional parking in accordance with this Chapter shall be provided for any alteration which would change the building square footage on site by more than 10 percent or 500 square feet, whichever is greater.
 3. Late night alcohol establishments. For late night alcohol establishments, additional parking in accordance with this Chapter shall be provided for any alteration what would change the building square footage on-site.
- E. When Constructed. Parking facilities required by this Chapter shall be constructed or installed prior to final inspection or the issuance of a Certificate of Occupancy for the uses that they serve.

(Ord. # 21-1436 §4, adopted 09/14/2021, effective 10/14/2021)

Effective on: 10/14/2021

17.44.020 Off-street parking—Residential uses.

The aggregate amount of off-street automobile parking spaces provided in connection with each of the following uses shall be not less than the following:

	Use	Parking Requirement
A.	One (1) family dwelling	Two (2) off-street parking spaces plus one (1) guest space.
B.	Duplex or two (2) family dwelling	Two (2) off-street parking spaces for each unit plus one (1) guest space. One (1) additional space of on-site guest parking shall be provided for each on-street space lost because of new curbcuts and/or driveways.
C.	Multiple dwellings (three (3) or more units)	Two (2) off-street spaces for each dwelling unit plus one (1) guest space for each two (2) dwelling units. One (1) additional space of on-site guest parking shall be provided for each on-street space lost because of new curbcuts and/or driveways.
D.	Detached servants’ quarters or guesthouses	One (1) space.
E.	Supportive or transitional housing, medical or residential care facilities, group homes: Limited to six (6) persons.	Same as one (1) family dwelling.
F.	Junior accessory dwelling unit and accessory dwelling unit	Refer to Section 17.21.050(F).

(Ord. 20-1404 §14, 2020; Ord. 20-1403U §14, 2020; Ord. 18-1385 §6, 2018; Ord. 13-1342, §11, #E, July 2013; prior code Appx. A, § 1151)

17.44.030 Off-street parking—Commercial and business uses.

Required Number of Spaces by Use. The aggregate amount of off-street automobile parking spaces provided for various uses shall not be less than the following, unless a parking plan approved by the planning commission pursuant to 17.44.210 allows for a reduction in the number of spaces required.

- A. Assembly halls: one (1) space for each five (5) seats, permanent or removable, or one (1) space for each fifty (50) square feet of gross floor area in the assembly hall, whichever is greater.
- B. Automobile or boat sales: one (1) space for each one thousand (1,000) square feet of site area.

- C. Bowling alleys: five (5) spaces for each lane plus one (1) space for each three hundred (300) square feet of gross floor area except bowling alley lanes and approach areas.
- D. Clubs, fraternity and sorority houses, rooming and boarding houses and similar uses having sleeping and guest rooms: two (2) covered spaces for each three (3) guest rooms; in dormitories each fifty (50) square feet shall be considered a guest room; two (2) spaces shall be required for each guest room with kitchen facilities.
- E. Commercial Uses.
 - 1. Bars and cocktail lounges: one (1) space for each eighty (80) square feet of gross floor area.
 - 2. Beauty colleges: one (1) space for each one hundred (100) square feet of gross floor area.
 - 3. Business schools and trade schools: one (1) space for each one hundred (100) square feet of gross floor area.
 - 4. Furniture and hardware stores: one (1) space for each two hundred fifty (250) square feet of gross floor area.
 - 5. Offices, general: one (1) space for each two hundred fifty (250) square feet of gross floor area.
 - 6. Offices, Governmental and Public Utilities. Government offices that generate high levels of contact with the public, or have high numbers of employees, including but not limited to employment offices, public social services offices, Department of Motor Vehicle offices: one (1) space per seventy-five (75) square feet of gross floor area for the first twenty-thousand (20,000) square feet of the building(s), plus one (1) space per two hundred fifty (250) square feet of gross floor area for the remaining floor area.
 - 7. Offices, medical: five (5) spaces for each one thousand (1,000) square feet of gross floor area.
 - 8. Restaurants: one (1) space for each one hundred (100) square feet of gross floor area plus one (1) per 200 square feet of on-site outdoor seating area in excess of 400 square feet.
 - 9. Retail, general retail commercial uses: one (1) space for each two hundred fifty (250) square feet of gross floor area.
 - 10. Gymnasiums/health and fitness centers, as follows:
 - a. Less than or equal to three thousand (3,000) square feet and with less than or equal to twenty (20) students at one time if classes are offered: one (1) space per two hundred fifty (250) square feet of gross floor area.
 - b. Greater than three thousand (3,000) square feet but not more than six thousand (6,000) square feet, or with more than forty (40) students at one time if classes are offered: one (1) space per two hundred (200) square feet of gross floor area.
 - c. Greater than six thousand (6,000) square feet, or with more than forty (40) students at one time if classes are offered: one (1) space per one hundred (100) square feet of gross floor area.
- F. Hospitals: two (2) spaces for each patient bed.
- G. Hospitals (mental), convalescent homes, guest homes, rest homes, sanitariums and similar institutions: one (1) space for each three (3) beds.
- H. Hotels: one (1) space for each unit for the first fifty (50) units; one (1) space per one (1) and one-half (1/2) units after fifty (50); and one (1) space per two (2) units after one hundred (100) units. Hotels with facilities including restaurants, banquet rooms, conference rooms, commercial retail uses and similar activities shall provide parking for the various uses as computed separately in accordance with the provisions of this chapter.
- I. Industrial Uses. The parking requirements of this subsection apply only to industrial uses; parking for commercial and other permitted uses in industrial zones shall provide the number of spaces as otherwise specified by this chapter.
 - 1. Industrial uses of all types, except, public utility facilities and warehouses: one (1) space for each vehicle used in conjunction with the use; plus one (1) space for each three hundred (300) square feet of gross floor area.

2. Warehouses, buildings or portions of buildings used exclusively for warehouse purposes: one (1) space for each one thousand (1,000) square feet for the first twenty thousand (20,000) square feet; plus, one (1) space for each two thousand (2,000) square feet for the second twenty thousand (20,000) square feet; plus one (1) space for each four thousand (4,000) square feet in excess of forty thousand (40,000) square feet; plus one (1) space for each vehicle operated from the property. Prior to approval of a warehouse use by the city, a covenant shall be recorded, guaranteeing the warehouse area, facility or building will not be converted, remodeled or changed to a nonwarehouse use unless the number of spaces otherwise required by this chapter are secured and provided prior to such change or unless approved by planning commission in accordance with this chapter.
- J. Mobilehomes or trailer parks: two (2) spaces for each dwelling unit with at least one (1) space adjacent to the trailer site.
- K. Mortuaries or undertaking establishments: one (1) space for each seventy-five (75) square feet of building area for the chapel or public assembly area.
- L. Motels: one (1) space for each unit, plus two (2) for the manager's unit.
- M. Recreation or amusement establishments: one (1) space for each seventy-five (75) square feet of gross floor area.
- N. Service stations: one (1) space for each one thousand (1,000) square feet of site area.
- O. Snack Bar/Snack Shop. The parking requirements for a snack bar and/or snack shop shall be the same as that for a restaurant.
- P. Short-term vacation rentals in commercial zones: one (1) space per bedroom, in no case less than one (1) space per unit and a maximum of two (2) spaces per unit being required. (Ord. 19-1395 §5, 2019; Ord. 17-1378 §7, 2017; Ord. 14-1346 §4, 2014; Ord. 04-1241 §4, 2004; Ord. 95-1126 §1, 1995; prior code Appx. A, § 1152)

17.44.040 Parking requirements for the downtown district.

The following requirements apply within the boundary of the downtown district, as defined by the map incorporated by this reference:

- A. The amount of parking shall be calculated for each particular use as set forth in Section [17.44.030](#) with the exception of the following:
 1. Retail, general retail commercial uses: one (1) space for each 333.33 square feet of gross floor area (or three (3) spaces per one thousand (1,000) square feet).
 2. Offices, general: one (1) space for each 333.33 square feet of gross floor area (or three (3) spaces per one thousand (1,000) square feet).
 3. Office, medical: one (1) space for each 333.33 square feet of gross floor area (or three (3) spaces per one thousand (1,000) square feet).
- B. Parking In-Lieu Fees. When the city council provides for contributions to an improvement fund for a vehicle parking district in lieu of parking spaces so required, said in-lieu fee contributions shall be considered to satisfy the requirements of this chapter pursuant to a parking plan approved by the planning commission.

(Ord. # [21-1436](#) §4, adopted 09/14/2021, effective 10/14/2021)

Effective on: 10/14/2021

17.44.050 Unlawful to reduce available parking.

The provision and maintenance of required off-street parking facilities and areas, and of area available to the owner or user of real property for meeting minimum required parking standards, shall be a continuing obligation of the property owner and user. An owner or user of real property containing uses for which off-street parking facilities or areas are required by this chapter shall be prohibited from the following:

- A. Reducing, diminishing or eliminating existing required off-street parking facilities or area under the ownership or control of such owner or user, whether on the same lot or on a separate lot from the use requiring such off-street parking facilities or area; or
- B. Selling, transferring, leasing or otherwise making unavailable for such required off-street parking facilities or area any portion of said lot or of any adjacent lot under the same ownership or control if the same is necessary for and available to satisfy in whole or in part the off-street parking requirements imposed by this chapter. (Prior code Appx. A, § 1153)

17.44.060 Common parking facilities.

Pursuant to a parking plan approved by the planning commission, common parking facilities may be provided to wholly or partially satisfy the off-street parking requirements of two (2) or more uses when one (1) or more of such uses will only infrequently generate use of such parking area at times when it will ordinarily be needed by the patrons or employees of the other use(s).

- A. The following factors shall be considered in determining the proportionate part of the required parking for such use(s):
 - 1. Whether the affected requirements are those of permanent buildings, or those of mere occupancies;
 - 2. The peak as well as normal days and hours of operation of such buildings and of the structures and occupancies with which it is proposed to share multiple-use parking areas;
 - 3. Whether the proposed multiple-use parking area is normally or frequently used by the patrons, customers or employees of other buildings or occupancies which will share such parking area at the same time as the applicant's patrons, customers and employees will normally or frequently utilize such parking area;
 - 4. The certainty that the multiple-use parking area(s) will be available for satisfying such parking requirements to the extent approved, and the permanency of such availability;
 - 5. The proximity and accessibility of the multiple-use parking area(s).
- B. A parking plan shall be so conditioned as to reasonably ensure the satisfaction of the appropriate parking requirements during the continued existence of the buildings or occupancies involved.
- C. If the common parking area(s) and the building sites to be served are subject to more than one ownership, permanent improvement and maintenance of such parking facilities must be provided in one of the following manners:
 - 1. By covenant or contract among all such property owners; and duly recording an appropriate covenant running with the land;
 - 2. By the creation of special districts and imposing of special assessments in any of the procedures prescribed by state law;
 - 3. By utilizing the authority vested in a parking authority as provided by state law;
 - 4. By dedicating such common parking area to the city for parking purposes subject to the acceptance of such dedication by the city council. (Prior code Appx. A, § 1154)

(Ord. # [21-1436](#) §4, adopted 09/14/2021, effective 10/14/2021)

Effective on: 10/14/2021

17.44.070 Off-street parking–Mixed uses.

Whenever there is a combination of two or more distinct uses on one lot or building site, the total number of parking spaces required to be provided for such lot or building site shall be not less than the sum total of the parking spaces required for each of the distinct uses. No off-street parking facilities provided for one use shall be deemed to provide parking facilities for any other use except as otherwise specified within this chapter or allowed pursuant to approval of a parking plan. (Prior code Appx. A, § 1155)

(Ord. # [21-1436](#) §4, adopted 09/14/2021, effective 10/14/2021)

Effective on: 10/14/2021

17.44.080 Uses not otherwise specified.

The aggregate amount of off-street automobile parking spaces provided in connection with any use not otherwise provided for in this chapter shall come before the commission for parking determination. (Prior code Appx. A, § 1156)

17.44.090 Off-street parking location.

All off-street automobile parking facilities shall be located as follows:

- A. All required parking spaces shall be located on the same lot or building site as the use for which such spaces are provided; provided however, that such parking spaces provided for commercial, business, industrial or warehouse uses may be located on a different lot or lots, all of which are less than three hundred (300) feet distance from the use for which it is provided, and such lot or lots are under common ownership with the lot or building site for which such spaces are provided.

Where the buildings are situated on one lot and the parking is situated on another lot, the owner shall file with the Community Development Department an affidavit recorded by the office of the Los Angeles County Recorder that these lots are held in common ownership for the use specified. Such distance shall be measured along a straight line drawn between the nearest point on the premises devoted to the use served by such parking facilities and the nearest point on the premises providing such parking facilities.

It is further provided that uses located within the boundaries of an established off-street parking district, organized pursuant to action by the City Council, shall be waived by the requirements of this subsection.

- B. No parking space required for any residential use shall be more than two hundred (200) feet total walking distance from the nearest entrance of the dwelling unit for which it is provided, except that residential uses located within the boundaries of an established off-street parking district, organized pursuant to action by the city council, shall be waived by the requirements of this subsection.
- C. In residential zones, garages or parking stalls fronting on a public street shall be set back a minimum of seventeen (17) feet from the exterior edge of the nearest public improvement (sidewalk or street improvement) if roll-up garage doors are installed, or set back twenty (20) feet if standard garage doors are installed. On streets where public improvements for sidewalks have not been completed the above setback shall be measured from the edge of the required or planned sidewalk. This measurement does not include structural supports or other parts of the structure provided parking dimension and turning radii are not obstructed.

Garages or parking stalls fronting on an alley shall provide one of the following setbacks from the property line: seventeen (17) feet, nine feet or three feet, except garages or parking stalls fronting on an alley of fifteen (15) feet in width or less need only to comply with the turning radius requirements of Section [17.44.130](#). For purposes of this section the service road located parallel to Hermosa Avenue approximately between 27th Street and 35th Street shall be considered as an alley.

- D. Residential parking within the front twenty (20) feet shall be allowed only when paved and leading to a garage.
- E. A garage may be located on one side lot line or on a rear property line which does not border a street or alley when said garage complies with all of the following:
 - 1. No portion of such garage is more than thirty-five (35) feet from the rear lot line; and
 - 2. No portion of such garage is closer than three feet to a habitable building on adjacent lot; and
 - 3. There are no openings on the side of the garage which are on the property line; and
 - 4. The wall on the side of the garage is constructed of one-hour fire resistant materials, and meets all building code regulations; and
 - 5. There has been provision for all roof drainage to be taken care of on the subject lot; and
 - 6. Such accessory structure is no more than one story in height and a distance of not less than six feet from the main building; and
 - 7. Such accessory building is used only for storage of automobiles, and may be used in conjunction therewith for open sun deck.
- F. Open parking spaces for residential uses in the open space zone (OS-O) shall be located only within the rear fifty (50) percent or in the rear forty (40) feet whichever is the lesser of a residential lot.
- G. Required guest parking spaces for duplex, two-family or multiple-family residential uses that are shared between units shall not be located in tandem and shall be open and accessible to guests of all the units. (Ord. 00-1207, §4 (part), 10/24/00; Ord. 98-1179, §4 (1), 01-27-98; Ord. 96-1153 § 1, 1996; Ord. 94-1120 § 1, 1994; prior code Appx. A, § 1157)

17.44.100 Size of spaces.

- A. No parking space for residential uses within any building shall be less than an inside dimension of eight feet, six inches wide or less than twenty (20) feet long.
- B. Parking spaces, not within a building, shall comply with the parking lot design standards attached hereto, with the following exceptions:
 - 1. In residential zones, guest parking spaces located in tandem behind a required parking space shall have a minimum length of seventeen (17) feet.
 - 2. Guest parking spaces situated parallel to alleys and located behind garage doors with a nine-foot setback shall have a minimum length of twenty-two (22) feet.
- C. Parking lot design standards for commercial and manufacturing uses are amended to allow the inclusion of thirty (30) percent compact car spaces in lots of ten or more stalls. (Prior code Appx. A, § 1158)

17.44.110 Tandem parking and entry-way standards.

- A. Residential Parking.
 - 1. No entranceway for vehicular access to any garage shall be less than eight feet wide. No such entranceway shall have less than six feet eight inches vertical clearance.
 - 2. In all residential zones, required parking spaces including replacement of on-street parking may be tandem. In the R-1 zone only, tandem parking may be accessed directly from a public street.
 - 3. Guest spaces in all residential zones may be located in garage setbacks of seventeen (17) feet or nine feet as required in Section 17.44.090(C); provided, they comply with the dimensional requirements specified in Section 17.44.100. However, in no case may one guest space be located behind another guest space.

4. The second floor level of a dwelling unit may project over a driveway fronting on a street or alley to within the prescribed setback required by the zone in which the development is proposed, or exists. (Prior code Appx. A, § 1159)
- B. Non-residential Parking. Tandem parking may be permitted to satisfy parking requirements for non-residential uses in accordance with the following.
 1. No more than two vehicles shall be placed one behind the other.
 2. Both spaces shall be assigned to a single non-residential establishment.

(Ord. # 21-1436 §4, adopted 09/14/2021, effective 10/14/2021)

Effective on: 10/14/2021

17.44.120 Driveways.

Off-street automobile parking facilities shall be provided with driveways providing vehicular access to such facilities from a public street or alley as follows:

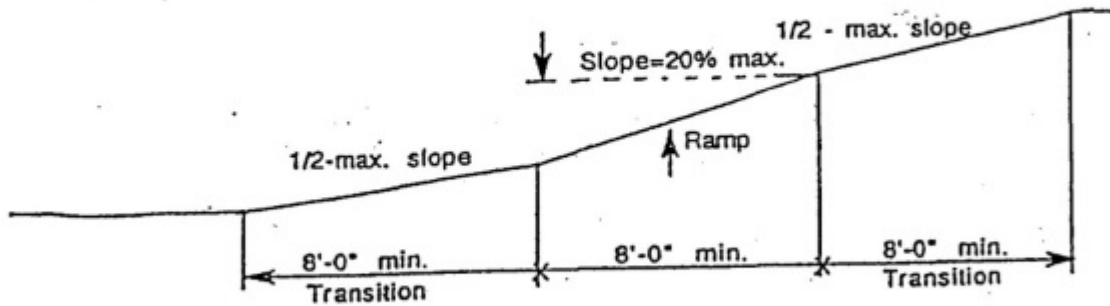
- A. Minimum driveway width shall be nine (9) feet, clear of all obstructions.
- B. Driveways and parking spaces shall be paved with not less than six (6) inches of portland cement concrete, except that when supported by a selected rock base which is acceptable to the chief building inspector for the type of soil upon which it is constructed, driveways may be paved with a minimum of three (3) inches of asphaltic concrete. Pervious concrete or similar material and drainage facilities may be alternatively installed for driveways and parking areas, or portions thereof, to specifications approved by the building official and/or city engineer as applicable. Where practicable, surface runoff shall drain into an adjacent pervious area on the property to maximize infiltration.
- C. Such driveways for vehicular access to parking spaces provided for any residential use shall be located wholly on the same lot as the parking spaces for which such driveway provides access, except in the case of common driveways. In the case of common driveways, easements of five feet on adjoining properties may be combined to create a driveway ten (10) feet in width.

Where access to required off-street parking spaces is via a common driveway, the owner shall file with the building department an affidavit recorded by the office of the Los Angeles County recorder that joint easements exist for the purpose of the driveway.

- D. No driveway providing access to any off-street parking space or garage shall have a slope greater than twenty (20) percent; provided, that any ramp slope in excess of twelve and one-half (12-1/2) percent includes transitions on each side with a minimum length of eight (8) feet and a maximum slope of one-half (1/2) the maximum ramp slope, in accordance with the driveway grade standards set forth below; further, any area used for guest parking shall have a maximum slope of twelve and one-half (12-1/2) percent. (Ord. 09-1300 § 4, May 2009; Ord. 93-1089 § 1, 1993; prior code Appx. A, § 1160)

Driveway Transitions

DRIVEWAY TRANSITIONS



NOTE:
No part of ramp shall encroach into the Public Right-of-Way

17.44.130 Turning radii, stall width and aisle width.

For the purpose of determining access to garages or open parking spaces, the minimum dimensions for turning radii, for stall widths, and for aisle widths shall be as set forth in the "parking lot design standards," on file with the city. Where an angle of parking other than one listed in the attached standards is proposed, the chief building inspector shall determine by interpolation the dimensions required for such parking. (Prior code Appx. A, § 1161)

[Repealed] 17.44.140 Requirements for new construction.

(Ord. # 21-1436 §4, adopted 09/14/2021, effective 10/14/2021)

Effective on: 10/14/2021

17.44.150 Underground parking facilities.

Underground parking facilities shall conform to all the provisions of this chapter; provided however, that underground parking facilities may be located in the side, front and rear yards which are completely below existing ground level. However, in the side yards and rear yards not abutting a street, the grade may be raised an average of three feet with a maximum of six feet above the existing grade, provided both side yards are provided with cement stops in order not to obstruct any pedestrian way. No portion of such facility shall have less than seven feet inside vertical clearance, except doorways may be six feet eight inches. (Prior code Appx. A, § 1163)

17.44.160 Required improvement and maintenance of parking area.

Every lot or area used for a public or private parking area shall be developed and maintained in the following manner:

- A. Surface Parking Area.

1. Off-street parking areas shall be paved with not less than three (3) inch asphaltic or six (6) inch portland cement concrete surfacing and maintained so as to eliminate dust or mud and shall be so graded and drained as to dispose of all surface water. Pervious material with drainage facilities may be alternatively installed for driveways and parking areas, or portions thereof, to specifications approved by the building official or city engineer as applicable. Where practicable, surface runoff shall drain into an adjacent pervious area on the property to maximize stormwater retention and filtration. In no case shall drainage be allowed across sidewalks or driveways, except residential use.
 2. Designated parking spaces shall be indicated with paint or approved stripping material on the surface of the parking area.
- B. Border Barricades, Screening and Landscaping.
1. Off-street parking area that is not separated by a fence from any street, alley or property line upon which it abuts, shall be provided with a suitable concrete curb or timber barrier of dressed dimension stock not less than six inches in height, located not less than two feet from such street or alley property lines, and such curb or barrier shall be securely installed and maintained; provided no such curb or barrier shall be required across any driveway or entrance to such parking area. Modifications for stormwater and urban runoff management (e.g., curb inlets) may be allowed to specifications approved by the building official or city engineer as applicable. Where practicable, surface runoff shall drain into an adjacent pervious area on the property to maximize infiltration.
 2. Any unenclosed off-street parking area abutting property located in one of the R zones shall be separated from such property by a solid masonry wall six (6) feet in height measured from the grade of the finished surface of such parking lot closest to the contiguous R zone property; provided, that along the required front yard, the solid masonry wall shall not exceed forty-two (42) inches in height. No such solid masonry wall need be provided where the elevation of that portion of the parking area immediately adjacent to an R zone is six (6) feet or more below the elevation of such R zone property along the common property line.
- C. Lighting. Light fixtures shall be high-efficiency, fully shielded (full cutoff) and down cast (emitting no light above the horizontal plane of the fixture), and not create glare or spill beyond the property lines. Any lights provided to illuminate any off-street parking area or used car sales area permitted by this ordinance shall be arranged so the light is reflected away from any street or premises upon which a dwelling unit is located and the lamp bulb is not directly visible from within any residential unit.
- D. Entrances and Exits. The location and design of all entrances and exits shall be subject to the approval of the city engineer.
- E. Traffic Circulation. Traffic circulation within off-street parking facilities except for residential parking shall be designed to ensure that no automobile need enter a major street in order to progress from one aisle to any other aisle within the same parking lot, or enter such major street backwards in order to leave such lot. If such circulation is not otherwise possible, a turnaround area within such lot, not less than thirty (30) feet in diameter, shall be provided. Directional signs or markings shall be provided in all facilities in which one-way traffic has been established.
- F. Authorized Vehicles. In all residential zones, parking spaces shall be maintained free and clear and utilized solely for the parking of authorized vehicles (obstructive storage prohibited).

"Authorized vehicles" shall mean automobiles, motorcycles, light trucks and vans not exceeding one and one-half ton capacity. Trailers, boats, recreational vehicles, motor homes, campers (not mounted to a motorized vehicle), tractor trucks and inoperable vehicles are prohibited. (Ord. 09-1300 §4, May 2009; Prior code Appx. A, § 1164)

17.44.170 Parking area in R-3 or R-P zones.

Every parking area located in an R-3 or R-P zone shall be governed by the following provisions in addition to those required above:

- A. No parking lot to be used as an accessory to a commercial or industrial establishment shall be established until it shall first have been reviewed by the planning commission and its location approved. Such approval may be conditioned upon the commission's required lighting, planting and/or maintenance of trees, shrubs or other landscaping within and along the borders of such parking area.
- B. Such a parking lot to be used as an accessory to a permitted commercial or industrial establishment shall be so located that the boundary of such parking lot closest to the site of the commercial or industrial establishment to which it is accessory shall be not more than fifty (50) feet distant.
- C. Such parking lot shall be used solely for the parking of private passenger vehicles.
- D. No sign of any kind, other than one designating entrances, exits or conditions of use shall be maintained on such parking lot. Any such sign shall not exceed eight square feet in area. (Prior code Appx. A, § 1165)

17.44.180 Resulting fractions.

When calculating the number of off-street automobile parking spaces required by this code for any particular use, building or structure, or integrated group of uses, buildings or structures, any resulting fraction less than one-half shall be disregarded, and any such fraction one-half or greater shall be construed as requiring one additional parking space. (Prior code Appx. A, § 1166)

17.44.190 Valet Parking.

Required off-street parking spaces for non-residential uses may be provided through valet parking with review and approval of a parking plan, in accordance with Section [17.44.210](#) (Parking plans).

(Ord. # [21-1436](#) §4, adopted 09/14/2021, effective 10/14/2021)

Effective on: 10/14/2021

17.44.200 Assignment of off-street residential parking spaces.

Required off-street parking spaces, except guest spaces, shall be permanently assigned and/or rented with each unit on the basis of the required parking per unit stated under Section [17.44.020](#), and the unit occupant shall be given sole use of said spaces for vehicle parking only. (Prior code Appx. A, § 1168)

17.44.210 Parking plans.

- A. A parking plan may be approved by the planning commission to allow for a reduction in the number of spaces required. The applicant shall provide the information necessary to show that adequate parking will be provided for customers, clients, visitors and employees or when located in a vehicle parking district, the applicant shall propose an in-lieu fee according to requirements of this chapter.
- B. Factors such as the following shall be taken into consideration:
 - 1. Van pools;
 - 2. Bicycle and foot traffic;
 - 3. Common parking facilities;
 - 4. Varied work shifts;
 - 5. Valet parking;
 - 6. Unique features of the proposed uses;

- 7. Peak hours of the proposed use as compared with other uses sharing the same parking facilities especially in the case of small restaurants or snack shops in the downtown area or in multitenant buildings;
 - 8. Mechanical vehicle lifts, not subject to Section 17.44.240(A)(2) and (3);
 - 9. Other methods of reducing parking demand.
- C. A covenant with the city a party thereto, may be required limiting the use of the property and/or designating the method by which the required parking will be provided at the time that the planning commission determines that inadequate parking exists.
- D. Fees, application and processing procedures for parking plans shall be set forth by resolution of the city council. (Ord. 20-1419 §4(2), 2020; Ord. 94-1099 § 3, 1994; prior code Appx. A, § 1169)

17.44.220 Consolidated off-street parking.

Subject to approval by the planning commission as prescribed in Section 17.44.210, required parking spaces for various uses may be reduced in number and computed at one space per two hundred fifty (250) square feet of gross floor area when parking is consolidated in retail shopping centers over ten thousand (10,000) square feet in size, or where public parking areas are created to take the place of on-site parking within vehicle parking districts. (Prior code Appx. A, § 1170)

17.44.230 Parking for reduced parking demand housing.

- A. When requested by the applicant, multi-family residential developments providing housing affordable to lower-income households, senior housing, and housing for disabled persons shall provide off-street parking according to the following formula:

Use	Off-Street Parking Spaces
Family housing (restricted to lower-income households)	
Studio	0.5 per unit
1-bedroom	1 per unit
2 or 3 bedrooms	2 per unit
4 or more bedrooms	3 per unit
Guest spaces	1 per 5 units
Staff member spaces	1 per 20 units
Senior housing or housing for disabled persons	
Studio	0.5 per unit
1 or 2 bedrooms	1 per unit
Guest spaces	1 per 5 units
Staff member spaces	1 per 20 units (senior housing)
	1 per 10 units (housing for disabled persons)
Single Room Occupancy (SRO) facility (restricted to lower-income persons)	
Studio	0.5
1-bedroom	1 per unit
Guest spaces	1 per 5 units
Staff (when applicable)	1 per 20 units

- B. The number of accessible parking spaces provided in accordance with Title 24 of the California Code of Regulations (California Building Standards Code) for Housing for Senior Citizens and housing for disabled persons shall be the number of spaces required in accordance with the basic parking ratio for multiple dwelling units.
- C. All required parking shall be provided in non-tandem parking spaces.
- D. Lower income housing: All units are rental units reserved for a period of at least 55 years for rental units restricted to lower-income households where affordable monthly rents shall not exceed 30% of 60% of annual median County household income divided by 12, and adjusted for household/unit size. SRO units shall be treated as one-bedroom units for the purposes of determining affordability. These restrictions shall be set forth in a written agreement between the property owner, and the City, the Housing Authority of the City Los Angeles or another housing provider approved by the City. These agreements shall specify: a) the maximum rents based on the same formula which established initial rent levels as a condition of City approval, or other formula approved by the City; b) the term for which rental units must remain affordable; and c) terms under which affordability is maintained after sale or transfer of the property.
- E. Housing for seniors or disabled persons: Applies to housing specifically restricted to, designed for and occupied by seniors or by disabled persons with limitations that affect the ability to drive.

17.44.240 Mechanical vehicle lifts.

- A. Building Permit Required.
 - 1. A building permit is required for the installation of a mechanical vehicle lift system.
 - 2. Mechanical vehicle lifts may be permitted to meet off-street parking space requirements in Section [17.44.020](#) (Off-street parking—Residential uses) on residential lots equal to or smaller than two thousand one hundred (2,100) square feet.
 - 3. On residential lots greater than two thousand one hundred (2,100) square feet, mechanical vehicle lifts are permitted where the parking space(s) provided by the mechanical vehicle lift(s) is/are in excess of the minimum number of required parking spaces and are subject to requirements of this section.
- B. Parking Plan Required. On residential lots greater than two thousand one hundred (2,100) square feet and in all other zones, mechanical vehicle lifts are allowed to meet the off-street parking space requirements in Sections [17.44.020](#) and [17.44.030](#) with review and approval of a parking plan, in accordance with Section [17.44.210](#) (Parking plans).
- C. Screening. Mechanical vehicle lift shall be located only within a fully enclosed garage.
- D. Vertical Clearance. A mechanical vehicle lift may only be used to store two (2) vehicles vertically where a minimum vertical height clearance from the garage floor to the garage ceiling plate or, in the case of a lift installed below the garage floor, from the below grade floor to the garage ceiling, is a minimum of twelve (12) feet clear of obstructions.
- E. Safety.
 - 1. All equipment shall be listed and rated by a testing agency recognized by California (i.e., UL).
 - 2. A mechanical vehicle lift shall be permitted only if it is operated with an automatic shutoff safety device and is installed in accordance with manufacturer specifications.
 - 3. A mechanical vehicle lift shall be equipped with a key locking mechanism.
 - 4. Mechanical vehicle lifts shall provide a manual override to access or remove vehicles from the mechanical vehicle lift in the event of a power outage.
- F. Miscellaneous.
 - 1. Mechanical vehicle lifts shall not be utilized to meet required guest parking. Guest parking shall remain open and accessible at all times.

2. In buildings that are nonconforming to parking, where fewer parking spaces are provided than required by Sections [17.44.020](#) and [17.44.030](#), the number of at-grade parking spaces shall not be reduced.
3. Standards in this chapter which are not specifically stated in and do not contradict this section still apply. (Ord. 20-1419 §4(3), 2020)



FINAL PARKING STUDY

HERMOSA BEACH PAVILION AT 1601 PACIFIC COAST HIGHWAY
CONVERSION OF 1,203 SF OF HALLWAY SPACE USE TO OFFICE/RETAIL USE

FEBRUARY 19, 2025

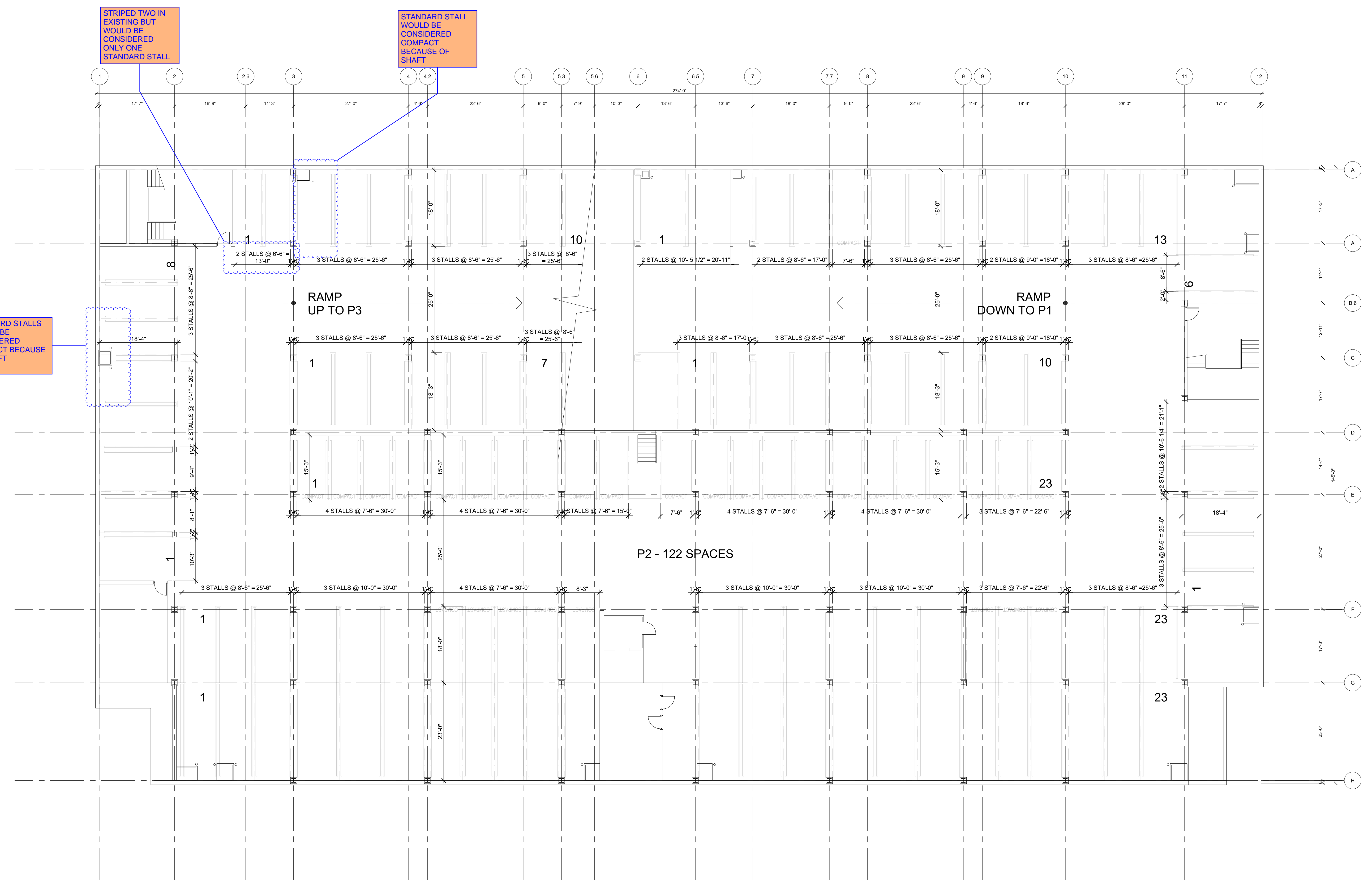
Appendix C

As-Built Parking Plans



J:\LOSANGELES\37-008917-00-180-L_PACIFIC_COAST_HWY_PARKING_CONSULTING\ACAD\05-ARCH\8917-AZ.1 P2 LEVEL FLOOR PLAN.DWG 4/2/2024 3:15:24 PM QUINITO, CHARINA

EXPIRATION DATE 11/30/2010
 DATE SEALED 12/31/2009
 PRINTED NAME & DISCIPLINE
 EOR / AOR LICENSE NO. XXXXXXXXXX



City of Hermosa Beach
 Community Development Department | Planning Division
 1315 Valley Drive, Hermosa Beach, CA 90254
 e: 310-318-0242 e: planning@hermosabeach.gov
 Office Hours: Monday - Thursday 7:00 AM - 6:00 PM

Table of Measurements				
E Angle (Degrees)	A Space Width Standard/Compact	B Standard/Compact	C Standard/Compact	D Aisle Width Standard/Compact
90°	8.5' / 7.5'	0' / 0'	18' / 15'	25' / 20'
60°	9.8' / 8.7'	11.5' / 10.2'	19.9' / 16.8'	18' / 14'
45°	12.0' / 10.6'	18.7' / 16.6'	18.7' / 15.9'	15' / 11'
30°	17.0' / 15.3'	28.4' / 25.4'	16.4' / 14.0'	11' / 10'

Notes:
 1. Distances are in feet.
 2. Parking lots/structures with 10 or more spaces are allowed up to 30% compact spaces.
 3. Space Width is measured parallel to drive aisle.
 4. Disabled persons spaces/improvements: Per Building Code.
 5. Turning radius may be reduced if stall width is increased. Variations shall be determined by the Community Development Director.

CAR COUNT							
TIER	STANDARD	STANDARD TANDEM	COMPACT	COMPACT TANDEM	CAR ACCESSIBLE	VAN ACCESSIBLE	TOTAL
P2 LEVEL	66	15	33	8	0	0	122
P3 LEVEL	65	10	35	8	0	0	118
P4 LEVEL	54	0	25	0	1	1	81
P5 LEVEL	58	0	33	0	1	1	93
P6 LEVEL	53	0	27	0	1	1	82
TOTAL	321	0	169	0	6	6	496
		COMPACT RATIO	34%				

1 P2 LEVEL FLOOR PLAN
 SCALE: 3/32" = 1'-0"

NOT FOR CONSTRUCTION

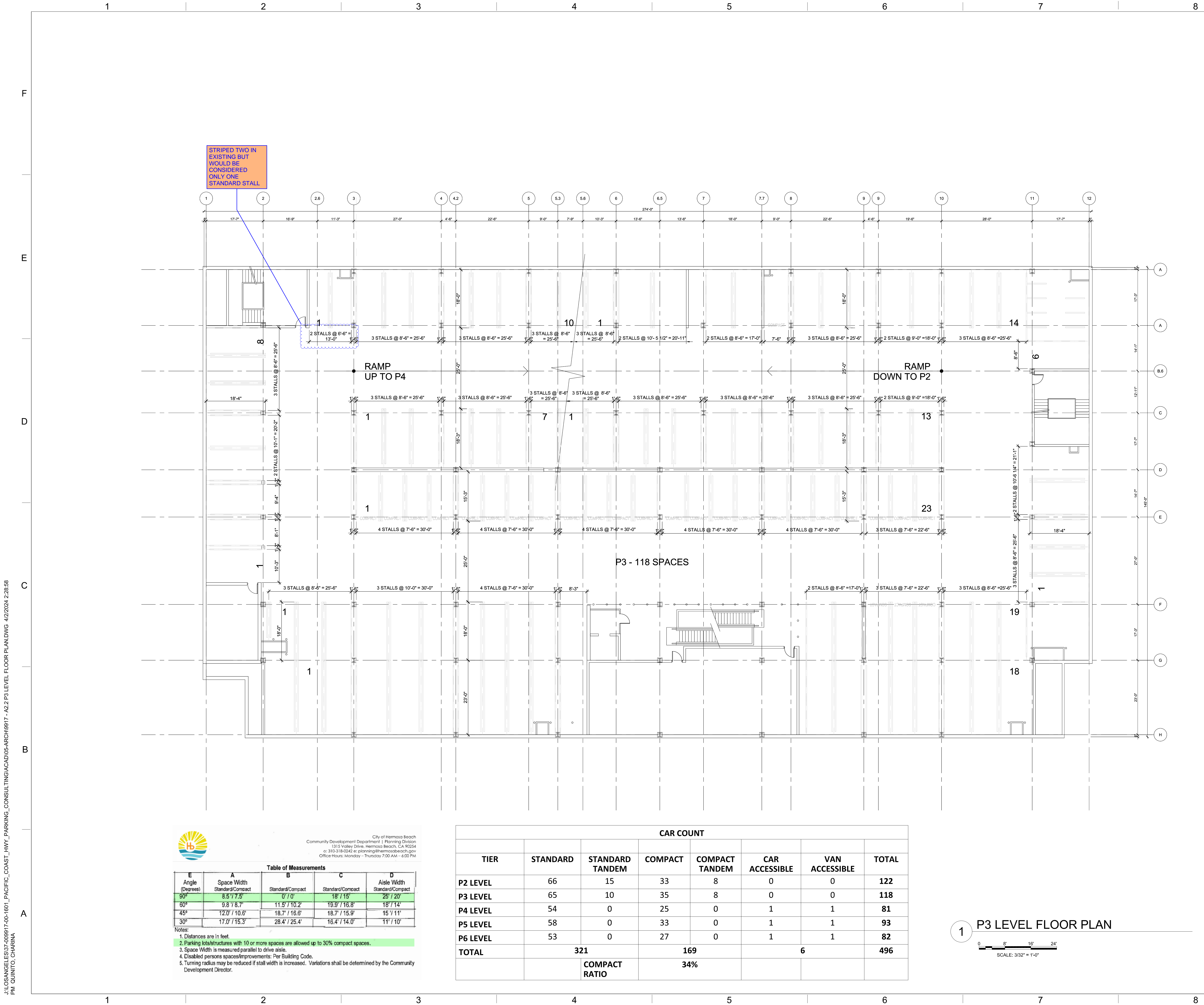
PROGRESS SET
 04/02/2024

MARK	DATE	DESCRIPTION
		ISSUE:
PROJECT NO:	37-00917-00	
DRAWN BY:		
CHECKED BY:		

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SHEET TITLE:
P2 LEVEL FLOOR PLAN

A2.1



J:\LOSANGELES\37-008917-001-1601-PACIFIC_COAST_HWY_PARKING_CONSULTING\ACAD\05-ARCH\0917 - A2.2 P3 LEVEL FLOOR PLAN.DWG 4/2/2024 2:28:58 PM QUINITO, CHARINA



City of Hermosa Beach
Community Development Department I Planning Division
1313 Valley Drive, Hermosa Beach, CA 90254
o: 310-318-0242 e: planning@hermosabeach.gov
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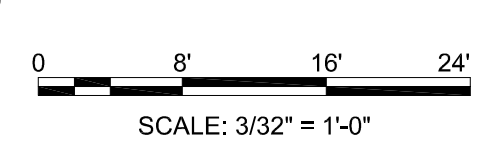
Table of Measurements				
E Angle (Degrees)	A Space Width Standard/Compact	B Standard/Compact	C Standard/Compact	D Aisle Width Standard/Compact
90°	8.5' / 7.5'	0' / 0'	18' / 15'	25' / 20'
60°	9.8' / 8.7'	11.5' / 10.2'	19.9' / 16.8'	18' / 14'
45°	12.0' / 10.6'	18.7' / 16.6'	18.7' / 15.9'	15' / 11'
30°	17.0' / 15.3'	28.4' / 25.4'	16.4' / 14.0'	11' / 10'

- Notes:
- Distances are in feet.
 - Parking lot structures with 10 or more spaces are allowed up to 30% compact spaces.
 - Space Width is measured parallel to drive aisle.
 - Disabled persons spaces/improvements: Per Building Code.
 - Turning radius may be reduced if stall width is increased. Variations shall be determined by the Community Development Director.

CAR COUNT

TIER	STANDARD	STANDARD TANDEM	COMPACT	COMPACT TANDEM	CAR ACCESSIBLE	VAN ACCESSIBLE	TOTAL
P2 LEVEL	66	15	33	8	0	0	122
P3 LEVEL	65	10	35	8	0	0	118
P4 LEVEL	54	0	25	0	1	1	81
P5 LEVEL	58	0	33	0	1	1	93
P6 LEVEL	53	0	27	0	1	1	82
TOTAL		321		169		6	496
		COMPACT RATIO		34%			

1 P3 LEVEL FLOOR PLAN



WALKER CONSULTANTS
606 South Olive Street
Suite 1100
Los Angeles, CA 90014
213.488.4911 Ph
www.walkerconsultants.com

Walker Parking Consultants / Engineers, Inc.
Firm Certificate of Authority No. XXXXXXXXXX

EXPIRATION DATE 11/30/2010
DATE SEALED 12/31/2009
PRINTED NAME & DISCIPLINE
EOR / AOR LICENSE NO. XXXXXXXXXX

**HERMOSA BEACH PAVILION
AS-BUILT PARKING PLANS**

1601 PACIFIC COAST HIGHWAY
HERMOSA BEACH
CALIFORNIA
90254

**PROGRESS SET
04/02/2024**

MARK	DATE	DESCRIPTION
		ISSUE:
PROJECT NO:	37-008917-00	
DRAWN BY:		
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SHEET TITLE:
P3 LEVEL FLOOR PLAN

A2.2

NOT FOR CONSTRUCTION

J:\LOSANGELES\37-008917-01-1601-PACIFIC_COAST_HWY_PARKING_CONSULTING\ACAD\05-ARCH\0917 - A2.3 P4 LEVEL FLOOR PLAN.DWG 4/2/2024 2:28:45 PM QUINITO, CHARINA

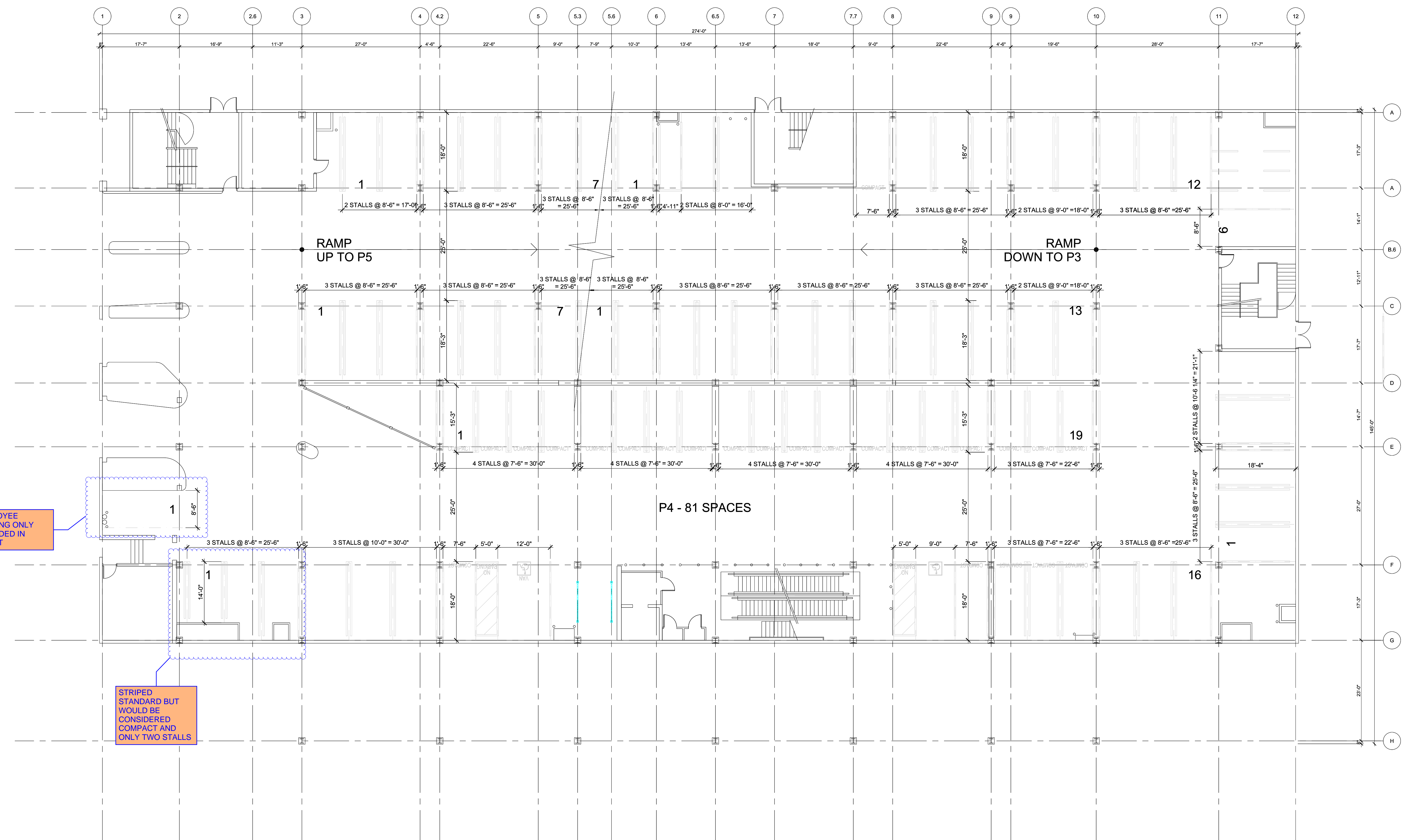


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City of Hermosa Beach
Community Development Department I Planning Division
1313 Valley Drive, Hermosa Beach, CA 90254
o: 310-318-0242 e: planning@hermosabeach.gov
Office Hours: Monday - Thursday 7:00 AM - 6:00 PM

Table of Measurements

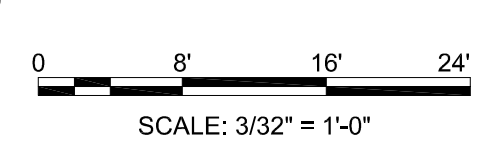
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- Notes:
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 - Parking kiosks/structures with 10 or more spaces are allowed up to 30% compact spaces.
 - Space Width is measured parallel to drive aisle.
 - Disabled persons spaces/improvements: Per Building Code.
 - Turning radius may be reduced if stall width is increased. Variations shall be determined by the Community Development Director.

CAR COUNT

TIER	STANDARD	STANDARD TANDEM	COMPACT	COMPACT TANDEM	CAR ACCESSIBLE	VAN ACCESSIBLE	TOTAL
P2 LEVEL	66	15	33	8	0	0	122
P3 LEVEL	65	10	35	8	0	0	118
P4 LEVEL	54	0	25	0	1	1	81
P5 LEVEL	58	0	33	0	1	1	93
P6 LEVEL	53	0	27	0	1	1	82
TOTAL		321	169		6		496
		COMPACT RATIO	34%				

1 P4 LEVEL FLOOR PLAN



NOT FOR CONSTRUCTION

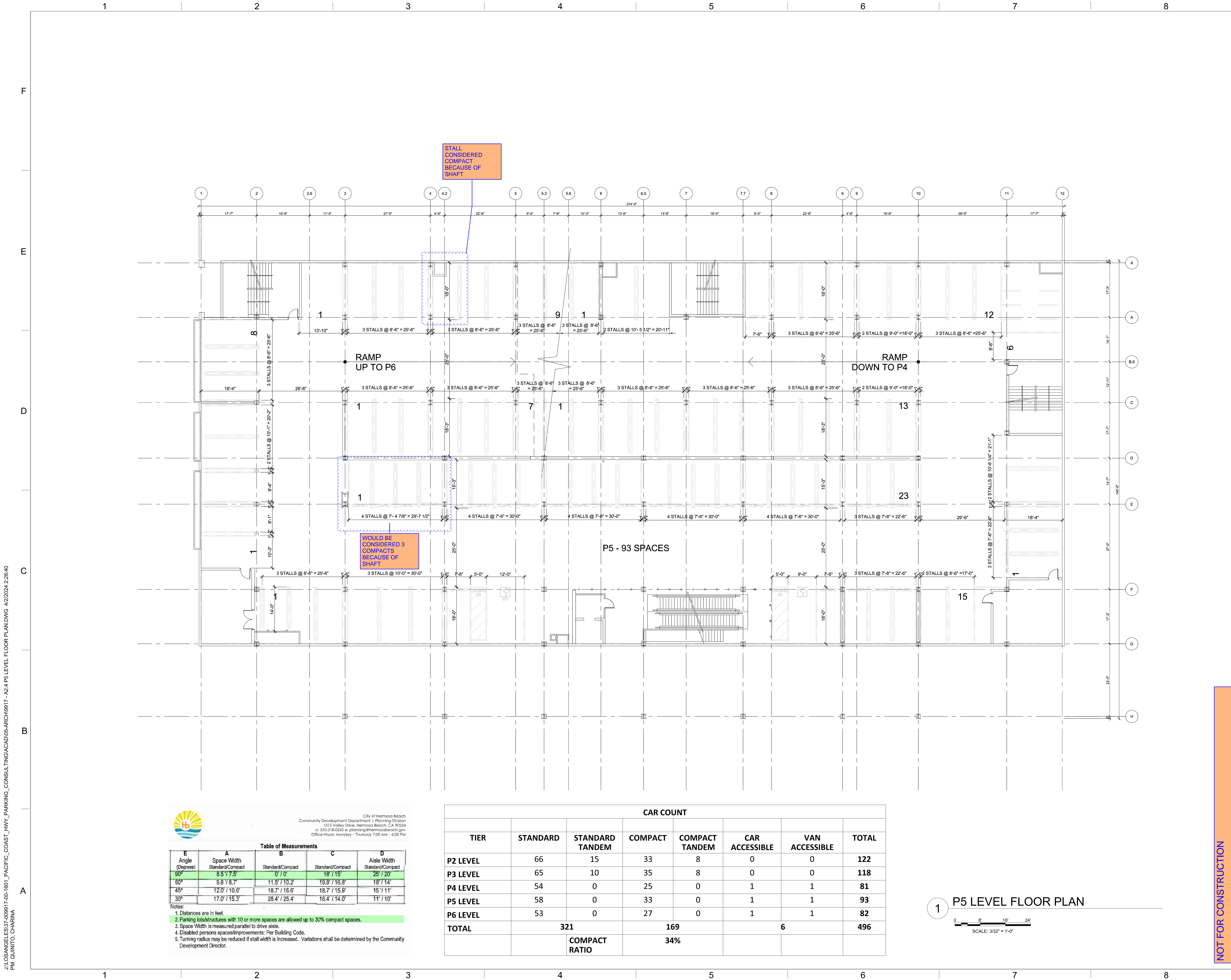
PROGRESS SET
04/02/2024

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		ISSUE:
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SHEET TITLE:
P4 LEVEL FLOOR PLAN

A2.3



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 1313 Valley Drive, Hermosa Beach, CA 90254
 o: 310-318-0242 e: planning@hermosabeach.gov
 Office Hours: Monday - Thursday 7:30 AM - 6:00 PM

Table of Measurements

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Notes:
 1. Distances are in feet.
 2. Parking kiosks/structures with 10 or more spaces are allowed up to 30% compact spaces.
 3. Space Width is measured parallel to drive aisle.
 4. Disabled persons spaces/improvements: Per Building Code.
 5. Turning radius may be reduced if stall width is increased. Variations shall be determined by the Community Development Director.

CAR COUNT

TIER	STANDARD	STANDARD TANDEM	COMPACT	COMPACT TANDEM	CAR ACCESSIBLE	VAN ACCESSIBLE	TOTAL
P2 LEVEL	66	15	33	8	0	0	122
P3 LEVEL	65	10	35	8	0	0	118
P4 LEVEL	54	0	25	0	1	1	81
P5 LEVEL	58	0	33	0	1	1	93
P6 LEVEL	53	0	27	0	1	1	82
TOTAL		321	169		6		496
		COMPACT RATIO	34%				

1 P5 LEVEL FLOOR PLAN

0 8' 16' 24'
 SCALE: 3/32" = 1'-0"

NOT FOR CONSTRUCTION

PROGRESS SET
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PROJECT NO:	37-008917-00	
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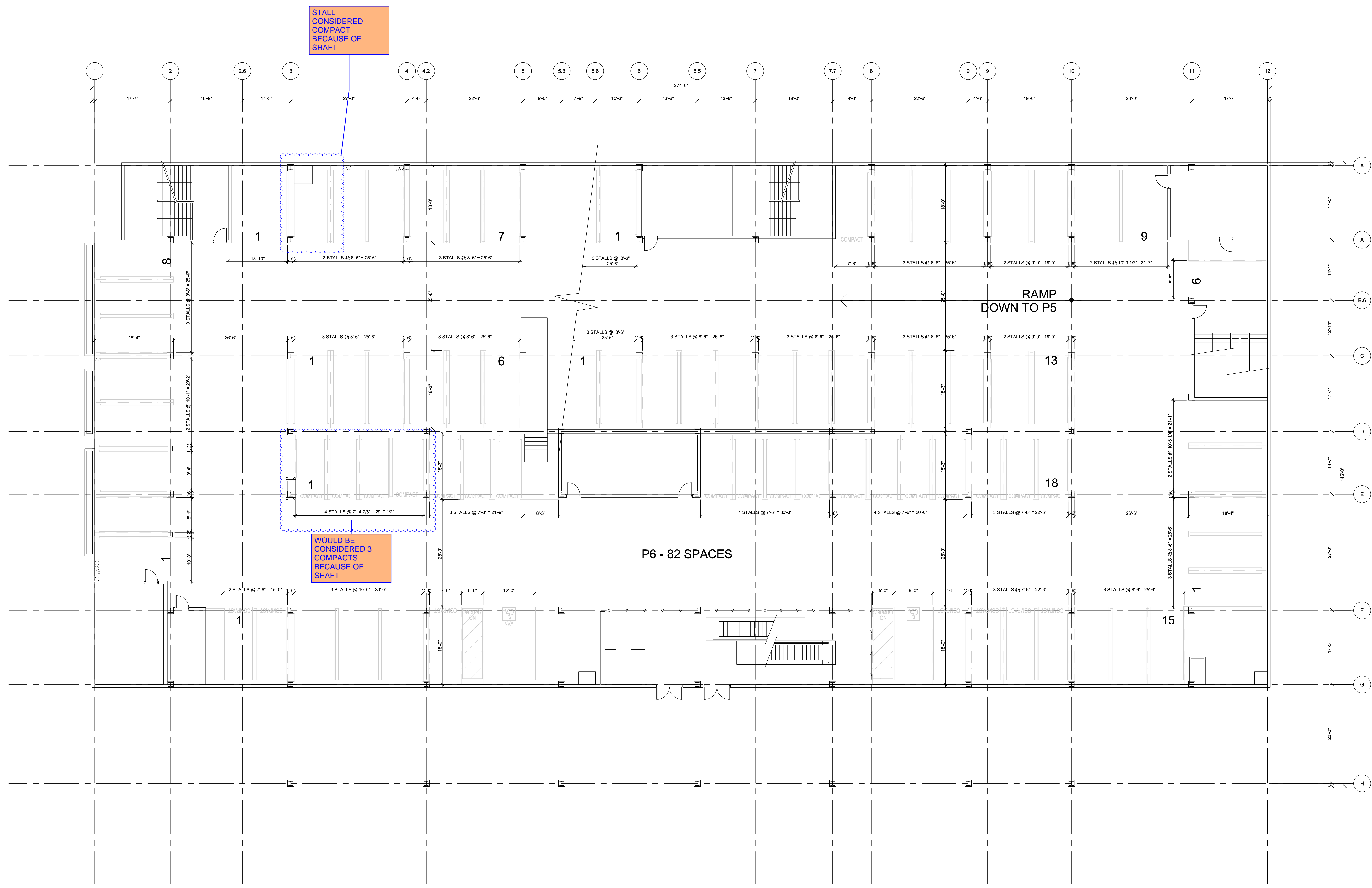
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P5 LEVEL FLOOR PLAN

A2.4

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EXPIRATION DATE 11/30/2010
 DATE SEALED 12/31/2009

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 o: 310-318-0242 e: planning@hermosabeach.gov
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 - Turning radius may be reduced if stall width is increased. Variations shall be determined by the Community Development Director.

CAR COUNT

TIER	STANDARD	STANDARD TANDEM	COMPACT	COMPACT TANDEM	CAR ACCESSIBLE	VAN ACCESSIBLE	TOTAL
P2 LEVEL	66	15	33	8	0	0	122
P3 LEVEL	65	10	35	8	0	0	118
P4 LEVEL	54	0	25	0	1	1	81
P5 LEVEL	58	0	33	0	1	1	93
P6 LEVEL	53	0	27	0	1	1	82
TOTAL		321		169		6	496
		COMPACT RATIO		34%			

1 P6 LEVEL FLOOR PLAN
 SCALE: 3/32" = 1'-0"

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SHEET TITLE:
P6 LEVEL FLOOR PLAN

A2.5



FINAL PARKING STUDY

HERMOSA BEACH PAVILION AT 1601 PACIFIC COAST HIGHWAY
CONVERSION OF 1,203 SF OF HALLWAY SPACE USE TO OFFICE/RETAIL USE

FEBRUARY 19, 2025

Appendix D

Parking Site Occupancy Data (Metropolis)



Location

Hermosa Pavilion Garage (Los Angeles, CA)

Visit Time	Capacity	Occupancies
1/15 12AM	528.0	96.0
Peak Occupancy	528.0	95.0
433.0	528.0	95.0
1/15 3AM	528.0	95.0
1/15 4AM	528.0	107.0
1/15 5AM	528.0	163.0
1/15 6AM	528.0	215.0
1/15 7AM	528.0	217.0
1/15 8AM	528.0	272.0
1/15 9AM	528.0	332.0
1/15 10AM	528.0	329.0
1/15 11AM	528.0	332.0
1/15 12PM	528.0	333.0
1/15 1PM	528.0	340.0
1/15 2PM	528.0	352.0
1/15 3PM	528.0	399.0
1/15 4PM	528.0	416.0
1/15 5PM	528.0	418.0
1/15 6PM	528.0	406.0
1/15 7PM	528.0	330.0
1/15 8PM	528.0	269.0
1/15 9PM	528.0	203.0
1/15 10PM	528.0	116.0
1/15 11PM	528.0	94.0
1/16 12AM	528.0	94.0
1/16 1AM	528.0	93.0
1/16 2AM	528.0	93.0
1/16 3AM	528.0	93.0
1/16 4AM	528.0	107.0
1/16 5AM	528.0	170.0
1/16 6AM	528.0	221.0
1/16 7AM	528.0	229.0

1/16 8AM	528.0	266.0
1/16 9AM	528.0	313.0
1/16 10AM	528.0	347.0
1/16 11AM	528.0	341.0
1/16 12PM	528.0	328.0
1/16 1PM	528.0	328.0
1/16 2PM	528.0	355.0
1/16 3PM	528.0	405.0
1/16 4PM	528.0	421.0
1/16 5PM	528.0	418.0
1/16 6PM	528.0	397.0
1/16 7PM	528.0	333.0
1/16 8PM	528.0	264.0
1/16 9PM	528.0	182.0
1/16 10PM	528.0	118.0
1/16 11PM	528.0	95.0
1/17 12AM	528.0	94.0
1/17 1AM	528.0	94.0
1/17 2AM	528.0	94.0
1/17 3AM	528.0	94.0
1/17 4AM	528.0	103.0
1/17 5AM	528.0	160.0
1/17 6AM	528.0	203.0
1/17 7AM	528.0	217.0
1/17 8AM	528.0	287.0
1/17 9AM	528.0	335.0
1/17 10AM	528.0	391.0
1/17 11AM	528.0	380.0
1/17 12PM	528.0	375.0
1/17 1PM	528.0	383.0
1/17 2PM	528.0	397.0
1/17 3PM	528.0	390.0
1/17 4PM	528.0	370.0
1/17 5PM	528.0	346.0

1/17 6PM	<u>528.0</u>	312.0
1/17 7PM	<u>528.0</u>	235.0
1/17 8PM	<u>528.0</u>	170.0
1/17 9PM	<u>528.0</u>	129.0
1/17 10PM	<u>528.0</u>	124.0
1/17 11PM	<u>528.0</u>	122.0
1/18 12AM	<u>528.0</u>	122.0
1/18 1AM	<u>528.0</u>	121.0
1/18 2AM	<u>528.0</u>	121.0
1/18 3AM	<u>528.0</u>	121.0
1/18 4AM	<u>528.0</u>	121.0
1/18 5AM	<u>528.0</u>	122.0
1/18 6AM	<u>528.0</u>	122.0
1/18 7AM	<u>528.0</u>	157.0
1/18 8AM	<u>528.0</u>	276.0
1/18 9AM	<u>528.0</u>	396.0
1/18 10AM	<u>528.0</u>	433.0
1/18 11AM	<u>528.0</u>	411.0
1/18 12PM	<u>528.0</u>	379.0
1/18 1PM	<u>528.0</u>	370.0
1/18 2PM	<u>528.0</u>	332.0
1/18 3PM	<u>528.0</u>	309.0
1/18 4PM	<u>528.0</u>	271.0
1/18 5PM	<u>528.0</u>	262.0
1/18 6PM	<u>528.0</u>	243.0
1/18 7PM	<u>528.0</u>	197.0
1/18 8PM	<u>528.0</u>	157.0
1/18 9PM	<u>528.0</u>	129.0
1/18 10PM	<u>528.0</u>	126.0
1/18 11PM	<u>528.0</u>	126.0
1/19 12AM	<u>528.0</u>	126.0
1/19 1AM	<u>528.0</u>	126.0
1/19 2AM	<u>528.0</u>	126.0
1/19 3AM	<u>528.0</u>	126.0

1/19 4AM	528.0	126.0
1/19 5AM	528.0	125.0
1/19 6AM	528.0	126.0
1/19 7AM	528.0	145.0
1/19 8AM	528.0	202.0
1/19 9AM	528.0	266.0
1/19 10AM	528.0	292.0
1/19 11AM	528.0	275.0
1/19 12PM	528.0	220.0
1/19 1PM	528.0	207.0
1/19 2PM	528.0	188.0
1/19 3PM	528.0	179.0
1/19 4PM	528.0	174.0
1/19 5PM	528.0	169.0
1/19 6PM	528.0	173.0
1/19 7PM	528.0	141.0
1/19 8PM	528.0	100.0
1/19 9PM	528.0	69.0
1/19 10PM	528.0	69.0
1/19 11PM	528.0	70.0
1/20 12AM	528.0	70.0
1/20 1AM	528.0	70.0
1/20 2AM	528.0	70.0
1/20 3AM	528.0	70.0
1/20 4AM	528.0	84.0
1/20 5AM	528.0	122.0
1/20 6AM	528.0	162.0
1/20 7AM	528.0	193.0
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1/20 9AM	528.0	259.0
1/20 10AM	528.0	269.0
1/20 11AM	528.0	266.0
1/20 12PM	528.0	276.0
1/20 1PM	528.0	280.0

1/20 2PM	528.0	295.0
1/20 3PM	528.0	296.0
1/20 4PM	528.0	325.0
1/20 5PM	528.0	303.0
1/20 6PM	528.0	278.0
1/20 7PM	528.0	224.0
1/20 8PM	528.0	196.0
1/20 9PM	528.0	141.0
1/20 10PM	528.0	96.0
1/20 11PM	528.0	75.0
1/21 12AM	528.0	74.0
1/21 1AM	528.0	74.0
1/21 2AM	528.0	74.0
1/21 3AM	528.0	75.0
1/21 4AM	528.0	89.0
1/21 5AM	528.0	152.0
1/21 6AM	528.0	220.0
1/21 7AM	528.0	216.0
1/21 8AM	528.0	250.0
1/21 9AM	528.0	318.0
1/21 10AM	528.0	319.0
1/21 11AM	528.0	338.0
1/21 12PM	528.0	336.0
1/21 1PM	528.0	350.0
1/21 2PM	528.0	371.0
1/21 3PM	528.0	394.0
1/21 4PM	528.0	407.0
1/21 5PM	528.0	393.0
1/21 6PM	528.0	382.0
1/21 7PM	528.0	323.0
1/21 8PM	528.0	281.0
1/21 9PM	528.0	213.0
1/21 10PM	528.0	132.0
1/21 11PM	528.0	104.0

1/22 12AM	528.0	104.0
1/22 1AM	528.0	105.0
1/22 2AM	528.0	105.0
1/22 3AM	528.0	104.0
1/22 4AM	528.0	118.0
1/22 5AM	528.0	191.0
1/22 6AM	528.0	251.0
1/22 7AM	528.0	255.0
1/22 8AM	528.0	277.0
1/22 9AM	528.0	338.0
1/22 10AM	528.0	346.0
1/22 11AM	528.0	344.0
1/22 12PM	528.0	329.0
1/22 1PM	528.0	348.0
1/22 2PM	528.0	361.0
1/22 3PM	528.0	386.0
1/22 4PM	528.0	392.0
1/22 5PM	528.0	390.0
1/22 6PM	528.0	369.0
1/22 7PM	528.0	314.0
1/22 8PM	528.0	268.0
1/22 9PM	528.0	198.0
1/22 10PM	528.0	122.0
1/22 11PM	528.0	90.0
1/23 12AM	528.0	90.0
1/23 1AM	528.0	90.0
1/23 2AM	528.0	90.0
1/23 3AM	528.0	91.0
1/23 4AM	528.0	107.0
1/23 5AM	528.0	159.0
1/23 6AM	528.0	203.0
1/23 7AM	528.0	210.0
1/23 8AM	528.0	257.0
1/23 9AM	528.0	317.0

1/23 10AM	528.0	332.0
1/23 11AM	528.0	350.0
1/23 12PM	528.0	338.0
1/23 1PM	528.0	320.0
1/23 2PM	528.0	352.0
1/23 3PM	528.0	364.0
1/23 4PM	528.0	375.0
1/23 5PM	528.0	389.0
1/23 6PM	528.0	371.0
1/23 7PM	528.0	323.0
1/23 8PM	528.0	268.0
1/23 9PM	528.0	188.0
1/23 10PM	528.0	123.0
1/23 11PM	528.0	88.0
1/24 12AM	528.0	88.0
1/24 1AM	528.0	86.0
1/24 2AM	528.0	86.0
1/24 3AM	528.0	86.0
1/24 4AM	528.0	101.0
1/24 5AM	528.0	153.0
1/24 6AM	528.0	199.0
1/24 7AM	528.0	219.0
1/24 8AM	528.0	281.0
1/24 9AM	528.0	375.0
1/24 10AM	528.0	396.0
1/24 11AM	528.0	394.0
1/24 12PM	528.0	373.0
1/24 1PM	528.0	380.0
1/24 2PM	528.0	360.0
1/24 3PM	528.0	356.0
1/24 4PM	528.0	329.0
1/24 5PM	528.0	327.0
1/24 6PM	528.0	314.0
1/24 7PM	528.0	246.0

1/24 8PM	528.0	175.0
1/24 9PM	528.0	131.0
1/24 10PM	528.0	122.0
1/24 11PM	528.0	122.0
1/25 12AM	528.0	116.0
1/25 1AM	528.0	116.0
1/25 2AM	528.0	116.0
1/25 3AM	528.0	116.0
1/25 4AM	528.0	117.0
1/25 5AM	528.0	120.0
1/25 6AM	528.0	119.0
1/25 7AM	528.0	144.0
1/25 8AM	528.0	254.0
1/25 9AM	528.0	364.0
1/25 10AM	528.0	409.0
1/25 11AM	528.0	400.0
1/25 12PM	528.0	385.0
1/25 1PM	528.0	348.0
1/25 2PM	528.0	328.0
1/25 3PM	528.0	299.0
1/25 4PM	528.0	272.0
1/25 5PM	528.0	246.0
1/25 6PM	528.0	228.0
1/25 7PM	528.0	178.0
1/25 8PM	528.0	128.0
1/25 9PM	528.0	102.0
1/25 10PM	528.0	100.0
1/25 11PM	528.0	99.0
1/26 12AM	528.0	99.0
1/26 1AM	528.0	99.0
1/26 2AM	528.0	99.0
1/26 3AM	528.0	99.0
1/26 4AM	528.0	99.0
1/26 5AM	528.0	98.0

1/26 6AM	528.0	98.0
1/26 7AM	528.0	113.0
1/26 8AM	528.0	187.0
1/26 9AM	528.0	292.0
1/26 10AM	528.0	316.0
1/26 11AM	528.0	300.0
1/26 12PM	528.0	236.0
1/26 1PM	528.0	199.0
1/26 2PM	528.0	198.0
1/26 3PM	528.0	195.0
1/26 4PM	528.0	181.0
1/26 5PM	528.0	162.0
1/26 6PM	528.0	151.0
1/26 7PM	528.0	125.0
1/26 8PM	528.0	78.0
1/26 9PM	528.0	46.0
1/26 10PM	528.0	48.0
1/26 11PM	528.0	49.0
1/27 12AM	528.0	49.0
1/27 1AM	528.0	49.0
1/27 2AM	528.0	49.0
1/27 3AM	528.0	50.0
1/27 4AM	528.0	63.0
1/27 5AM	528.0	122.0
1/27 6AM	528.0	168.0
1/27 7AM	528.0	191.0
1/27 8AM	528.0	218.0
1/27 9AM	528.0	246.0
1/27 10AM	528.0	257.0
1/27 11AM	528.0	272.0
1/27 12PM	528.0	284.0
1/27 1PM	528.0	292.0
1/27 2PM	528.0	318.0
1/27 3PM	528.0	347.0

1/27 4PM	528.0	358.0
1/27 5PM	528.0	380.0
1/27 6PM	528.0	378.0
1/27 7PM	528.0	314.0
1/27 8PM	528.0	234.0
1/27 9PM	528.0	176.0
1/27 10PM	528.0	98.0
1/27 11PM	528.0	68.0
1/28 12AM	528.0	68.0
1/28 1AM	528.0	67.0
1/28 2AM	528.0	68.0
1/28 3AM	528.0	69.0
1/28 4AM	528.0	84.0
1/28 5AM	528.0	165.0
1/28 6AM	528.0	220.0
1/28 7AM	528.0	222.0
1/28 8AM	528.0	284.0
1/28 9AM	528.0	347.0
1/28 10AM	528.0	334.0
1/28 11AM	528.0	338.0
1/28 12PM	528.0	322.0
1/28 1PM	528.0	341.0
1/28 2PM	528.0	349.0
1/28 3PM	528.0	355.0
1/28 4PM	528.0	364.0
1/28 5PM	528.0	374.0
1/28 6PM	528.0	354.0
1/28 7PM	528.0	290.0
1/28 8PM	528.0	221.0
1/28 9PM	528.0	162.0
1/28 10PM	528.0	111.0
1/28 11PM	528.0	81.0
1/29 12AM	528.0	81.0
1/29 1AM	528.0	80.0

1/29 2AM	528.0	81.0
1/29 3AM	528.0	82.0
1/29 4AM	528.0	98.0
1/29 5AM	528.0	166.0
1/29 6AM	528.0	234.0
1/29 7AM	528.0	253.0
1/29 8AM	528.0	270.0
1/29 9AM	528.0	291.0
1/29 10AM	528.0	299.0
1/29 11AM	528.0	320.0
1/29 12PM	528.0	321.0
1/29 1PM	528.0	335.0
1/29 2PM	528.0	348.0
1/29 3PM	528.0	355.0
1/29 4PM	528.0	356.0
1/29 5PM	528.0	377.0
1/29 6PM	528.0	344.0
1/29 7PM	528.0	308.0
1/29 8PM	528.0	268.0
1/29 9PM	528.0	196.0
1/29 10PM	528.0	127.0
1/29 11PM	528.0	82.0
1/30 12AM	528.0	81.0
1/30 1AM	528.0	82.0
1/30 2AM	528.0	86.0
1/30 3AM	528.0	85.0
1/30 4AM	528.0	96.0
1/30 5AM	528.0	144.0
1/30 6AM	528.0	196.0
1/30 7AM	528.0	212.0
1/30 8AM	528.0	266.0
1/30 9AM	528.0	296.0
1/30 10AM	528.0	300.0
1/30 11AM	528.0	311.0

1/30 12PM	528.0	323.0
1/30 1PM	528.0	326.0
1/30 2PM	528.0	346.0
1/30 3PM	528.0	369.0
1/30 4PM	528.0	363.0
1/30 5PM	528.0	360.0
1/30 6PM	528.0	334.0
1/30 7PM	528.0	296.0
1/30 8PM	528.0	230.0
1/30 9PM	528.0	160.0
1/30 10PM	528.0	103.0
1/30 11PM	528.0	76.0
1/31 12AM	528.0	75.0
1/31 1AM	528.0	75.0
1/31 2AM	528.0	75.0
1/31 3AM	528.0	75.0
1/31 4AM	528.0	90.0
1/31 5AM	528.0	145.0
1/31 6AM	528.0	191.0
1/31 7AM	528.0	202.0
1/31 8AM	528.0	262.0
1/31 9AM	528.0	338.0
1/31 10AM	528.0	384.0
1/31 11AM	528.0	372.0
1/31 12PM	528.0	370.0
1/31 1PM	528.0	367.0
1/31 2PM	528.0	355.0
1/31 3PM	528.0	339.0
1/31 4PM	528.0	345.0
1/31 5PM	528.0	342.0
1/31 6PM	528.0	318.0
1/31 7PM	528.0	239.0
1/31 8PM	528.0	157.0
1/31 9PM	528.0	114.0

1/31 10PM	528.0	107.0
1/31 11PM	528.0	105.0
2/1 12AM	528.0	95.0
2/1 1AM	528.0	95.0
2/1 2AM	528.0	94.0
2/1 3AM	528.0	89.0
2/1 4AM	528.0	90.0
2/1 5AM	528.0	93.0
2/1 6AM	528.0	92.0
2/1 7AM	528.0	114.0
2/1 8AM	528.0	233.0
2/1 9AM	528.0	342.0
2/1 10AM	528.0	378.0
2/1 11AM	528.0	374.0
2/1 12PM	528.0	345.0
2/1 1PM	528.0	345.0
2/1 2PM	528.0	334.0
2/1 3PM	528.0	309.0
2/1 4PM	528.0	280.0
2/1 5PM	528.0	245.0
2/1 6PM	528.0	228.0
2/1 7PM	528.0	179.0
2/1 8PM	528.0	136.0
2/1 9PM	528.0	112.0
2/1 10PM	528.0	107.0
2/1 11PM	528.0	107.0
2/2 12AM	528.0	106.0
2/2 1AM	528.0	106.0
2/2 2AM	528.0	106.0
2/2 3AM	528.0	106.0
2/2 4AM	528.0	106.0
2/2 5AM	528.0	108.0
2/2 6AM	528.0	107.0
2/2 7AM	528.0	124.0

2/2 8AM	<u>528.0</u>	182.0
2/2 9AM	<u>528.0</u>	260.0
2/2 10AM	<u>528.0</u>	270.0
2/2 11AM	<u>528.0</u>	265.0
2/2 12PM	<u>528.0</u>	257.0
2/2 1PM	<u>528.0</u>	256.0
2/2 2PM	<u>528.0</u>	235.0
2/2 3PM	<u>528.0</u>	212.0
2/2 4PM	<u>528.0</u>	201.0
2/2 5PM	<u>528.0</u>	195.0
2/2 6PM	<u>528.0</u>	175.0
2/2 7PM	<u>528.0</u>	152.0
2/2 8PM	<u>528.0</u>	100.0
2/2 9PM	<u>528.0</u>	76.0
2/2 10PM	<u>528.0</u>	73.0
2/2 11PM	<u>528.0</u>	72.0
2/3 12AM	<u>528.0</u>	72.0
2/3 1AM	<u>528.0</u>	72.0
2/3 2AM	<u>528.0</u>	72.0
2/3 3AM	<u>528.0</u>	73.0
2/3 4AM	<u>528.0</u>	90.0
2/3 5AM	<u>528.0</u>	153.0
2/3 6AM	<u>528.0</u>	199.0
2/3 7AM	<u>528.0</u>	208.0
2/3 8AM	<u>528.0</u>	241.0
2/3 9AM	<u>528.0</u>	282.0
2/3 10AM	<u>528.0</u>	275.0
2/3 11AM	<u>528.0</u>	272.0
2/3 12PM	<u>528.0</u>	275.0
2/3 1PM	<u>528.0</u>	291.0
2/3 2PM	<u>528.0</u>	281.0
2/3 3PM	<u>528.0</u>	300.0
2/3 4PM	<u>528.0</u>	348.0
2/3 5PM	<u>528.0</u>	373.0

2/3 6PM	528.0	339.0
2/3 7PM	528.0	265.0
2/3 8PM	528.0	218.0
2/3 9PM	528.0	140.0
2/3 10PM	528.0	65.0
2/3 11PM	528.0	43.0
2/4 12AM	528.0	43.0
2/4 1AM	528.0	43.0
2/4 2AM	528.0	43.0
2/4 3AM	528.0	43.0
2/4 4AM	528.0	62.0
2/4 5AM	528.0	133.0
2/4 6AM	528.0	188.0
2/4 7AM	528.0	190.0
2/4 8AM	528.0	238.0
2/4 9AM	528.0	284.0
2/4 10AM	528.0	289.0
2/4 11AM	528.0	287.0
2/4 12PM	528.0	302.0
2/4 1PM	528.0	319.0
2/4 2PM	528.0	332.0
2/4 3PM	528.0	359.0
2/4 4PM	528.0	372.0
2/4 5PM	528.0	372.0
2/4 6PM	528.0	328.0
2/4 7PM	528.0	277.0
2/4 8PM	528.0	221.0
2/4 9PM	528.0	167.0
2/4 10PM	528.0	106.0
2/4 11PM	528.0	74.0
2/5 12AM	528.0	74.0
2/5 1AM	528.0	74.0
2/5 2AM	528.0	75.0
2/5 3AM	528.0	76.0

2/5 4AM	<u>528.0</u>	93.0
2/5 5AM	<u>528.0</u>	159.0
2/5 6AM	<u>528.0</u>	221.0
2/5 7AM	<u>528.0</u>	244.0
2/5 8AM	<u>528.0</u>	267.0
2/5 9AM	<u>528.0</u>	316.0
2/5 10AM	<u>528.0</u>	314.0
2/5 11AM	<u>528.0</u>	330.0
2/5 12PM	<u>528.0</u>	342.0
2/5 1PM	<u>528.0</u>	296.0
2/5 2PM	<u>528.0</u>	237.0
2/5 3PM	<u>528.0</u>	233.0



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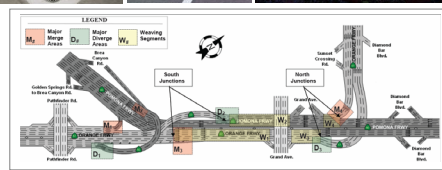
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	2019 Winner of the Orange County Engineering Council's Outstanding Service Award	
	2016 Winner of the ASCE's Outstanding Civil Engineer in the Private Sector Award in the State of California	
	2016 Winner of the ASCE Los Angeles Section's Outstanding Civil Engineer in the Private Sector Award	
	2016 Winner of the ASCE Orange County Chapter's Outstanding Civil Engineer in the Private Sector Award	
	2016 Certificate of Recognition for Dedication to Support the ELTP Program by Los Angeles County MTA/Metro	
	2016 Winner of the Orange County Engineering Council's Outstanding Engineering Service Award	
	2015 Orange County Business Journal's 2015 Excellence in Entrepreneurship Award Nominee	
	2014 Orange County Business Journal's 2014 Excellence in Entrepreneurship Award Nominee	
	2012 Winner of Cal-EPA/California Air Resources Board's Cool California Climate Leader	
	2011 Award of Excellence in Service by Los Angeles County MTA/Metro in the County of Los Angeles	
	2011 Award of Excellence in Service by Los Angeles County MTA/Metro in the County of Los Angeles	
	2010 Award of Excellence in Service by Los Angeles County MTA/Metro in the County of Los Angeles	
	2009 Winner of the ASCE's Outstanding Private Sector Civil Engineering Project in Metropolitan Los Angeles	
	2009 Winner of the Caltrans' 2009 Excellence in Transportation Award in the State of California	
	2007 Winner of the ASCE's Outstanding Public/Private Sector Civil Engineering Project in Metropolitan Los Angeles	
	2005 Winner of the APWA's Best Traffic Congestion Mitigation Project of the Year in Southern California	
	2004 Top Nominee of Transportation Foundation's Highway Management Program in the State of California	
	2003 Winner of the PTI's Best Transportation Technology Solutions Award in the United States	
	2002 Winner of the ITS-CA's Best Return on Investment Project Award in the State of California	
	2000 Award of Excellence in Service by Los Angeles County MTA/Metro in the County of Los Angeles	



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